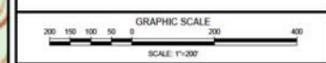


**SHEET INDEX PLAN**  
SCALE: 1"=400'

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TOWN OF  
KILLINGWORTH,  
CONNECTICUT

**PARKER HILL ROAD  
AND EXTENSION  
DRAINAGE  
IMPROVEMENTS**

**SHEET INDEX AND  
GENERAL NOTES**

**CONTRACT DRAWINGS**

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REVISIONS	
No.	DATE

DATE: FEBRUARY 17, 2015  
SCALE: AS NOTED  
PROJECT No.: 00550077  
CADD FILE: 00550077SP  
DESIGNED: ALM  
DRAWN: AJS  
CHECKED: JHP

SHEET No.:  
**2 OF 17**

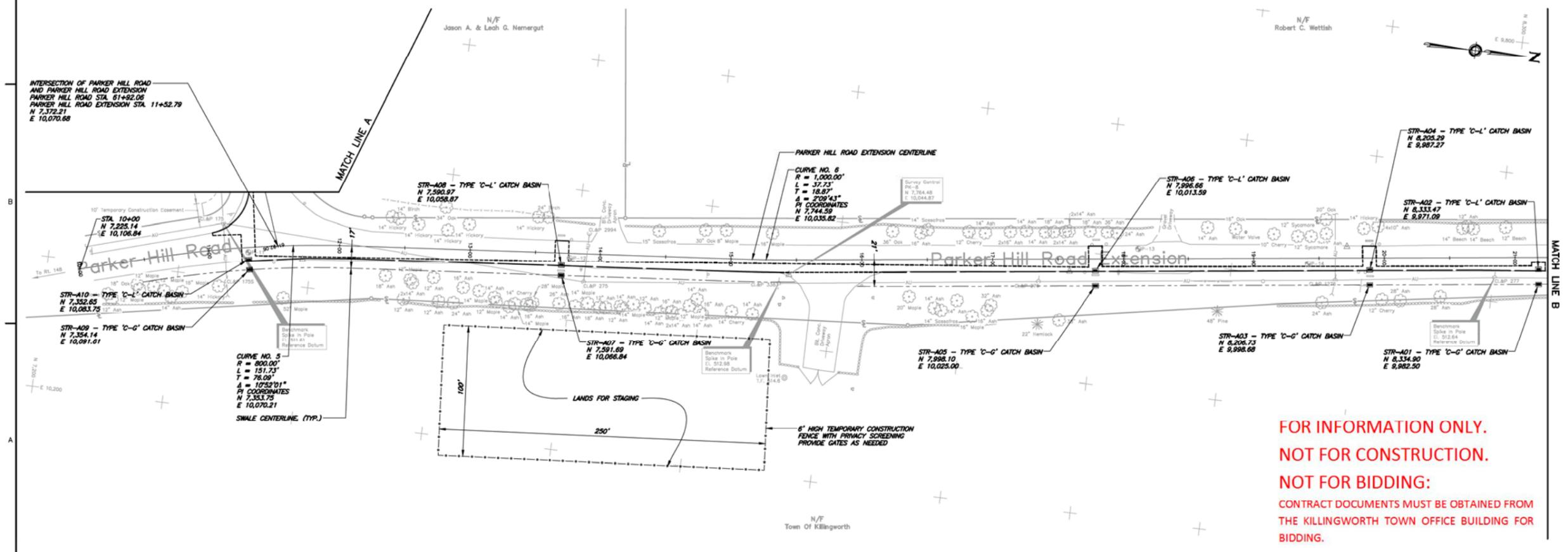
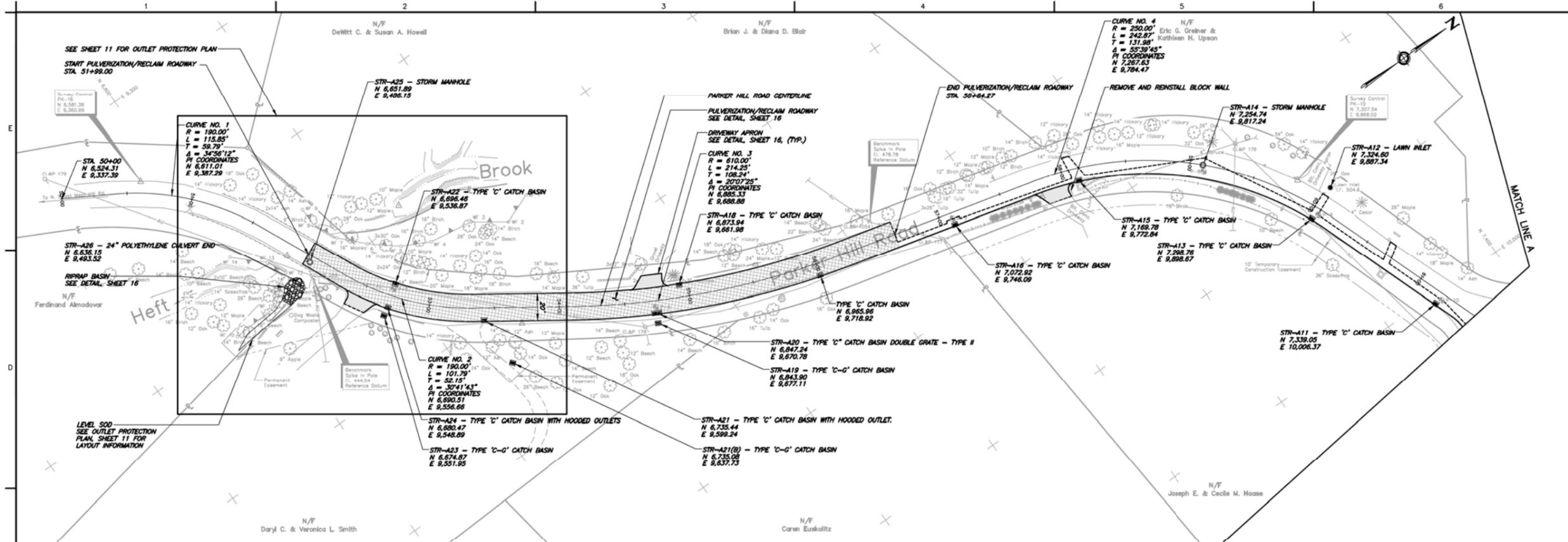
**GENERAL INFORMATION**

- THE SITE IS LOCATED WITHIN THE CONNECTICUT WATER COMPANY PUBLIC WATER SUPPLY WATERSHED.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN OIL ABSORBENT SPILL RESPONSE BOOMS AND BLANKETS ON SITE FOR THE ENTIRE CONSTRUCTION PERIOD.
- NO CONSTRUCTION VEHICLES WILL BE STORED, SERVICED, REFUELED, WASHED, OR FLUSHED OUT IN A LOCATION WHERE LEAKS, SPILLAGE, WASTE MATERIALS, CLEANERS, OR WATERS WILL BE INTRODUCED OR FLOW INTO WETLANDS OR WATERCOURSES.
- NO EQUIPMENT STORAGE, CLEANING, REPAIRING OR REFUELING SHALL BE CONDUCTED WITHIN 200' OF AN INLAND WETLAND BOUNDARY.
- DURING CONSTRUCTION, ROUTINE EQUIPMENT MAINTENANCE AND REFUELING SHALL OCCUR ON AN IMPERVIOUS SURFACE WITH OIL ABSORBENT SPILL RESPONSE MATERIALS IN PLACE. NON-ROUTINE MAINTENANCE OF EQUIPMENT SHALL BE CONDUCTED OFF-SITE.
- SHOULD ANY CONTAMINANT SPILL OCCUR, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE CONNECTICUT DEPARTMENT OF ENERGY AND ENVIRONMENTAL PROTECTION, OIL AND CHEMICAL SPILL RESPONSE DIVISION AT 860-424-3338, CONNECTICUT WATER COMPANY AT 860-669-8636, AND THE TOWN.
- BULK FUEL SUPPLY FOR CONSTRUCTION PURPOSES SHALL BE STORED OFF-SITE.
- INSTALL EROSION CONTROLS AROUND THE BASE OF ALL STOCKPILES AND TEMPORARILY SEED OR COVER THE PILES WITH AN IMPERVIOUS COVER IF IT IS TO REMAIN ON THE SITE LONGER THAN ONE MONTH.
- DURING CONSTRUCTION, THE CONTRACTOR SHALL STORE ALL HAZARDOUS MATERIALS OFF-SITE, OR IF ON-SITE, THEN WITHIN A SECONDARY CONTAINMENT STRUCTURE WITH AN IMPERVIOUS FLOOR, THAT WILL BE SECURED DURING NON-WORKING HOURS.
- AT LEAST ONE (1) WEEK PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE TOWN AND THE CONNECTICUT WATER COMPANY.
- AT LEAST THREE (3) BUSINESS DAYS PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE TOWN ZONING ENFORCEMENT OFFICER. EROSION AND SEDIMENT CONTROL INSTALLATION (NOT DESIGN) SHALL BE INSPECTED AND APPROVED BY THE ZONING ENFORCEMENT OFFICER BEFORE FURTHER CONSTRUCTION CAN COMMENCE.
- SURPLUS OR UNSUITABLE MATERIALS SHALL BE DISPOSED OF OFF-SITE IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL ORDINANCES, RULES, REGULATIONS AND CODES.
- PAVEMENTS TO BE CUT SHALL BE SAW CUT PRIOR TO PAVING. CLEAN FACE OF EXISTING PAVEMENT AND PAINT WITH LIQUID BITUMEN. MATCH EXISTING GRADES WITH NEW PAVEMENT.
- CONTRACTOR SHALL REMOVE TREES AND OTHER WOODY VEGETATION AND GRUB STUMPS AS NECESSARY FOR THE CONSTRUCTION. SITE DISTURBANCE TO BE KEPT TO A MINIMUM. ALL TREES SHALL BE PROTECTED UNLESS OTHERWISE SHOWN OR AUTHORIZED BY THE ENGINEER. TREES TO BE REMOVED SHALL BE INDIVIDUALLY VERIFIED IN THE FIELD WITH THE ENGINEER.
- SEE SHEET 12 OF 17 FOR PROJECT NARRATIVE AND CONSTRUCTION SCHEDULING NOTES.
- PROTECT AND MAINTAIN ALL EXISTING UTILITIES LOCATED WITHIN THE VICINITY OF THE CONSTRUCTION SITE, UNLESS OTHERWISE NOTED. IF ANY UTILITY IS DAMAGED OR SERVICE IS INTERRUPTED DURING CONSTRUCTION, THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ALL DAMAGES INCLUDING RESTORING SERVICE IN A SAFE MANNER, TO THE COMPLETE SATISFACTION OF THE UTILITY OWNER AND THE ENGINEER.
- INFORMATION OR DATA SHOWN ON OR INDICATED IN THE CONTRACT DOCUMENTS WITH RESPECT TO EXISTING UNDERGROUND PIPES, CABLES, CONDUITS, STRUCTURES OR OTHER UNDERGROUND FACILITIES IS BELIEVED TO BE REASONABLY CORRECT BUT IS NOT GUARANTEED TO BE EXACT OR COMPLETE. SUCH INFORMATION SHALL BE CONSIDERED TO HAVE BEEN PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR AND TO ALERT THE CONTRACTOR TO THE EXISTENCE OF SUCH UNDERGROUND FACILITIES WITHIN OR CONTIGUOUS TO THE PROJECT SITE AND THE OWNER AND ENGINEER SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ANY SUCH INFORMATION OR DATA.
- THE CONTRACTOR SHALL HAVE FULL RESPONSIBILITY FOR REVIEWING AND CHECKING ALL INFORMATION AND DATA DESCRIBED IN NOTE 17 ABOVE, FOR LOCATING ALL SUCH UNDERGROUND FACILITIES SHOWN OR INDICATED IN THE CONTRACT DOCUMENTS, FOR COORDINATION OF THE WORK WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES DURING CONSTRUCTION, FOR THE SAFETY AND PROTECTION THEREOF, AND FOR REPAIRING ANY DAMAGE THERE TO RESULTING FROM THE WORK. THE COST OF ALL OF WHICH WILL BE CONSIDERED AS HAVING BEEN INCLUDED IN THE CONTRACT PRICE.
- THE CONTRACTOR SHALL BE LIABLE FOR ALL DAMAGES AND CLAIMS RECEIVED OR SUSTAINED BY ANY PERSONS, CORPORATIONS OR PROPERTY IN CONSEQUENCE OF THE DAMAGE TO EXISTING UTILITIES, ROADWAYS, THEIR APPURTENANCES, OR OTHER FACILITIES CAUSED DIRECTLY OR INDIRECTLY BY THE OPERATIONS OF THE CONTRACTOR.
- NEATLY REMOVE, STORE AND PROTECT AS APPLICABLE, AND REINSTALL OR REPLACE AS APPLICABLE, EXISTING SITE FEATURES DISTURBED BY CONSTRUCTION, REQUIRED FOR FINISHED CONSTRUCTION.
- PROPERLY CONTROL MUFFLERS AND NOISE CONTROL DEVICES. REPLACE DEFECTIVE DEVICES AS NECESSARY AND LOCATED DEWATERING PUMPS AS FAR AS POSSIBLE FROM RESIDENTIAL STRUCTURES SO AS TO MINIMIZE THE AMOUNT OF NOISE GENERATED AT THE SITE THAT WILL REACH RESIDENTIAL STRUCTURES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTROL OF WATER TO ENABLE CONSTRUCTION IN THE DRY, INCLUDING BUT NOT LIMITED TO TRENCHES, EXCAVATIONS, WATER CONTROL STRUCTURES AND COFFERDAMS, THAT MAY BE REQUIRED TO PROPERLY COMPLETE THE WORK. PARTICULAR ATTENTION IS CALLED TO FLUCTUATIONS IN WATER FLOWS AND LEVELS THAT MAY OCCUR DUE TO PRECIPITATIONS EVENTS. NO EXTRA COMPENSATION WILL BE ALLOWED DUE TO WATER FLOW OR LEVEL FLUCTUATIONS. WHETHER PUMPING OR SIPHONING FOR DEWATERING IS USED OR NOT, IN ALL CASES, THE DISCHARGE SHALL BE HANDLED SO AS TO AVOID EROSION AND SEDIMENTATION, AS APPROVED BY THE ENGINEER. TAKE ALL NECESSARY PRECAUTIONS AND FURNISH EQUIPMENT REQUIRED TO HANDLE ALL SURFACE, SUBSURFACE AND FLOOD FLOWS WHICH MAY BE ENCOUNTERED AT ANY TIME DURING CONSTRUCTION.
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- IF TOWN AND PRIVATE ROADWAYS ARE IMPACTED WITH TRACKED SOILS, CONTRACTOR SHALL SWEEP CLEAN AT ALL TIMES AND AS DIRECTED BY THE ENGINEER.

**REFERENCES**

- GROUND BASED TOPOGRAPHIC AND BOUNDARY SURVEY PERFORMED BY LAND SURVEY & TECHNICAL SERVICES, INC. THE SURVEY FOR PARKER HILL ROAD AT ROUTE 148 INTERSECTION WAS PERFORMED IN JUNE 2007. THE SURVEY FOR THE REMAINING PORTION OF PARKER HILL ROAD AND FOR PARKER HILL ROAD EXTENSION WAS PERFORMED IN SEPTEMBER AND OCTOBER 2014. THE VERTICAL AND HORIZONTAL DATUMS FOR EACH SURVEY ARE ASSUMED.
- THIS SURVEY WAS PREPARED PURSUANT TO THE REGULATIONS OF CONNECTICUT STATE AGENCIES SECTIONS 20-300b-1 THROUGH 20-300b-20 AS A ACCURACY CLASS 1-2.
- INLAND WETLAND SOILS WERE FLAGGED IN THE FIELD BY RICHARD SNARSKI, CPSS #1975, NEW ENGLAND ENVIRONMENTAL SERVICES, 155 JERRY DANIELS ROAD, MARLBOROUGH, CT 06447. FLAGS WERE LOCATED IN THE FIELD BY LAND SURVEY & TECHNICAL SERVICES, INC.
- PROBES WERE PERFORMED BY CLARENCE WELTI ASSOCIATES, INC. ON OCTOBER 24, 2014.

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  - INSTALL SEDIMENTATION CONTROL AT CATCH BASIN IN ALL PROPOSED CATCH BASINS, SEE DETAIL, SHEET 13.
  - PROVIDE KNIFE EDGES AT ALL BITUMINOUS CONCRETE CURB ENDS. SEE DETAIL, SHEET 16.



TOWN OF  
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PARKER HILL ROAD  
AND EXTENSION  
DRAINAGE  
IMPROVEMENTS

LAYOUT PLAN

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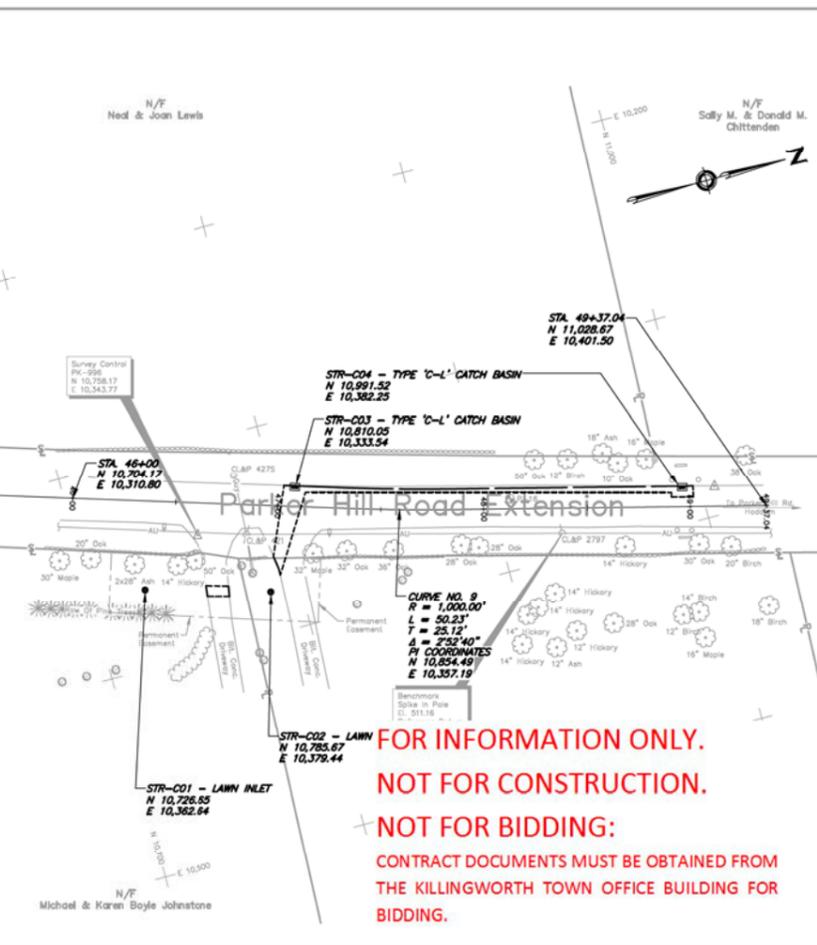
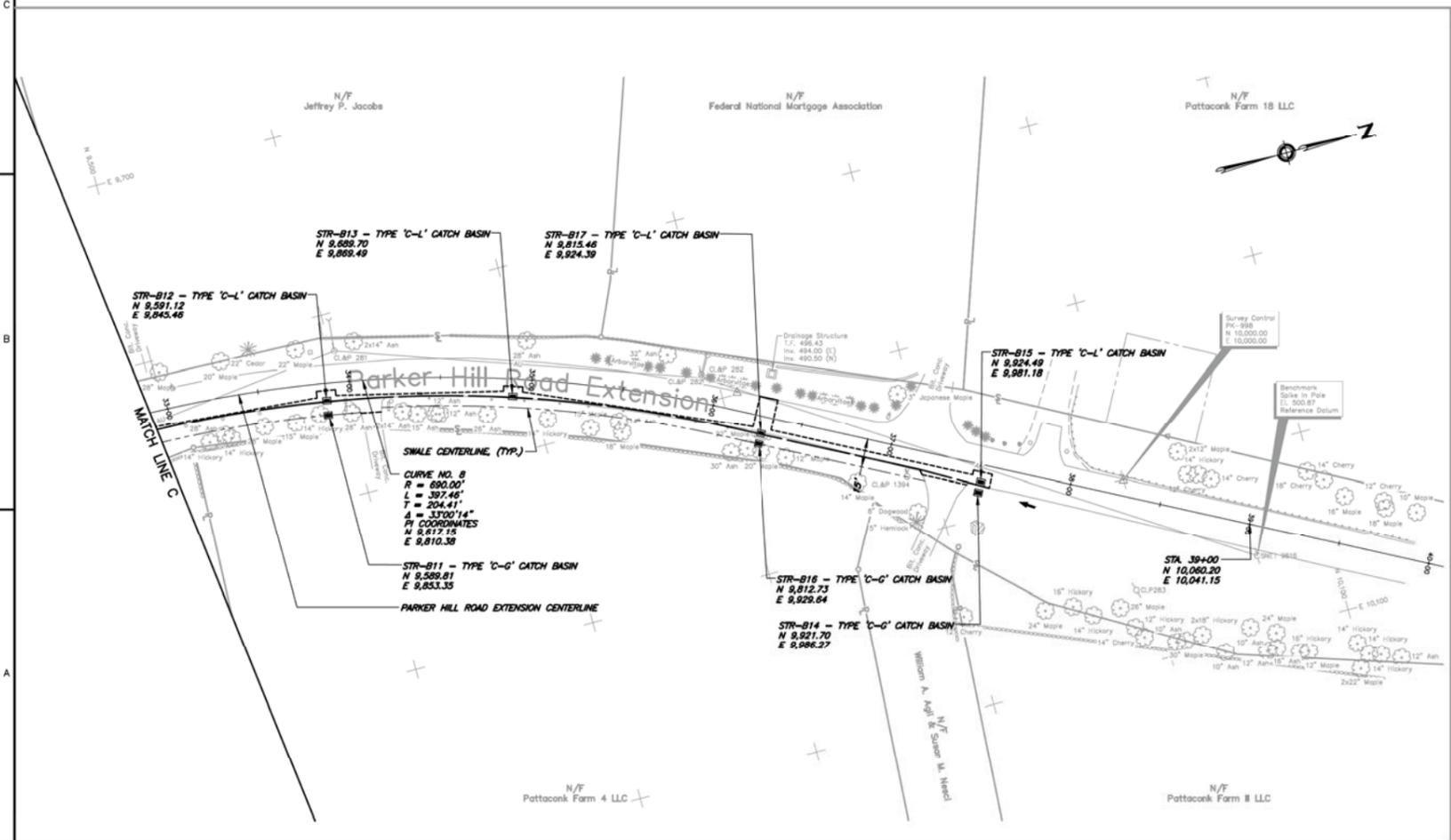
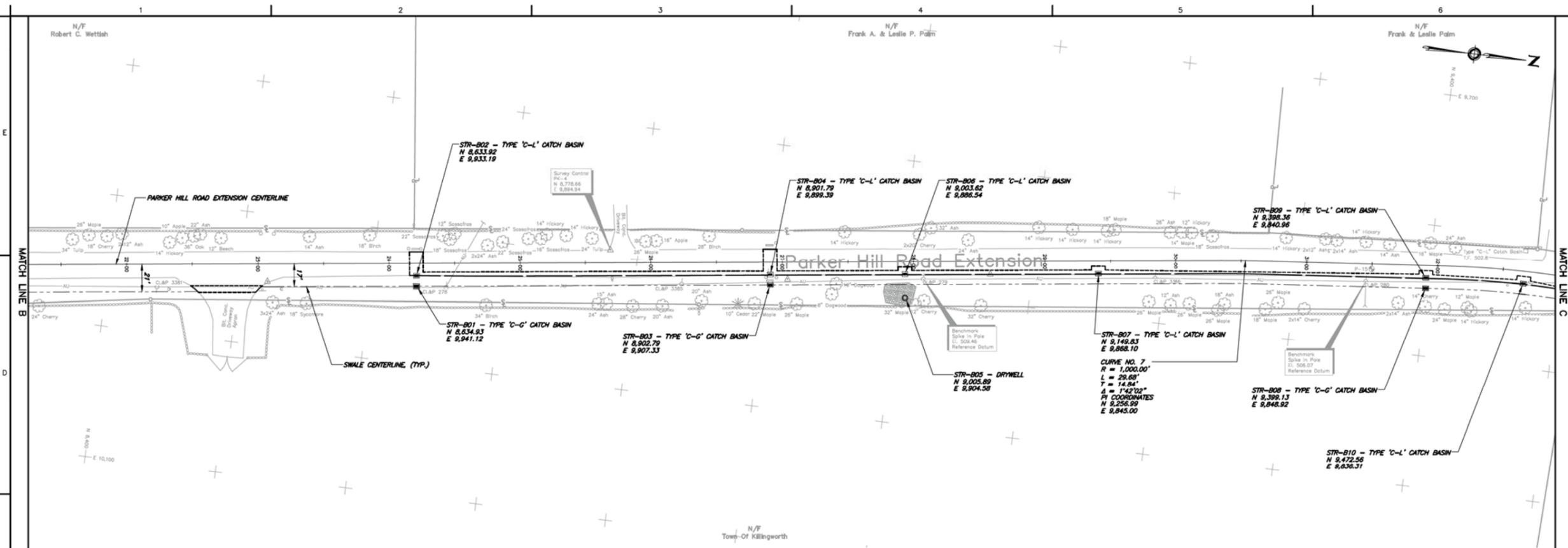
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REVISIONS

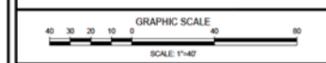
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 SCALE: 1"=40'  
 PROJECT No.: 00550077  
 CADD FILE: 00550077SP  
 DESIGNED: ALM  
 DRAWN: AJG  
 CHECKED: JJP

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  - PROVIDE KNIFE EDGES AT ALL BITUMINOUS CONCRETE CURB ENDS. SEE DETAIL, SHEET 16.



TOWN OF  
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PARKER HILL ROAD  
AND EXTENSION  
DRAINAGE  
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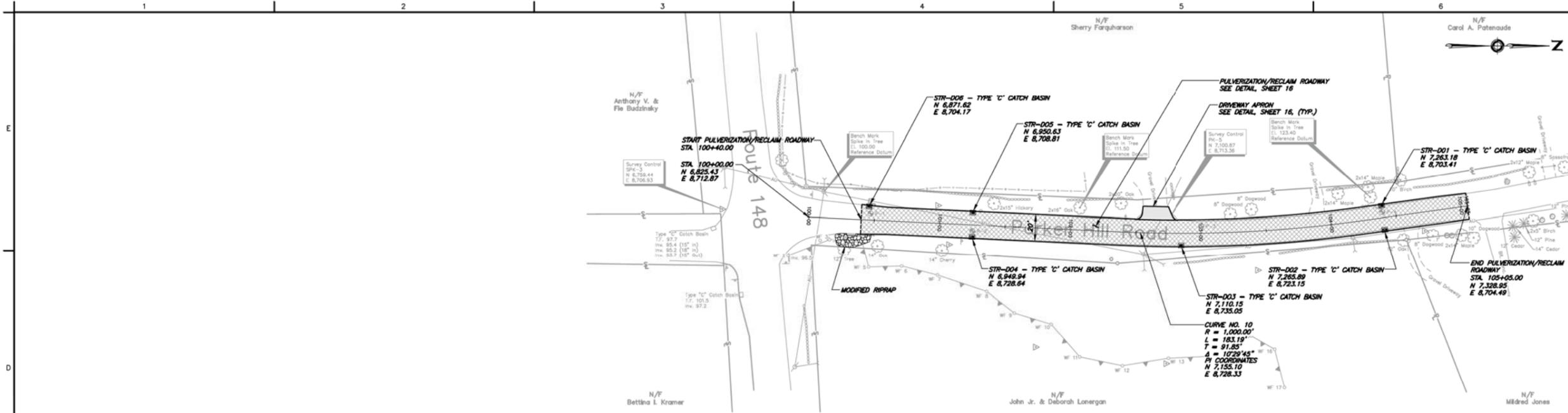
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No.	DESCRIPTION	DATE

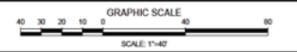
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DRAWN: AJG  
CHECKED: JJP

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**4 OF 17**

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  - VERTICAL AND HORIZONTAL CONTROL ON THIS SHEET IS INDEPENDENT FROM CONTROL PROVIDED ON SHEETS 3 AND 4.
  - PROVIDE KNIFE EDGES AT ALL BITUMINOUS CONCRETE CURB ENDS. SEE DETAIL, SHEET 16.



TOWN OF  
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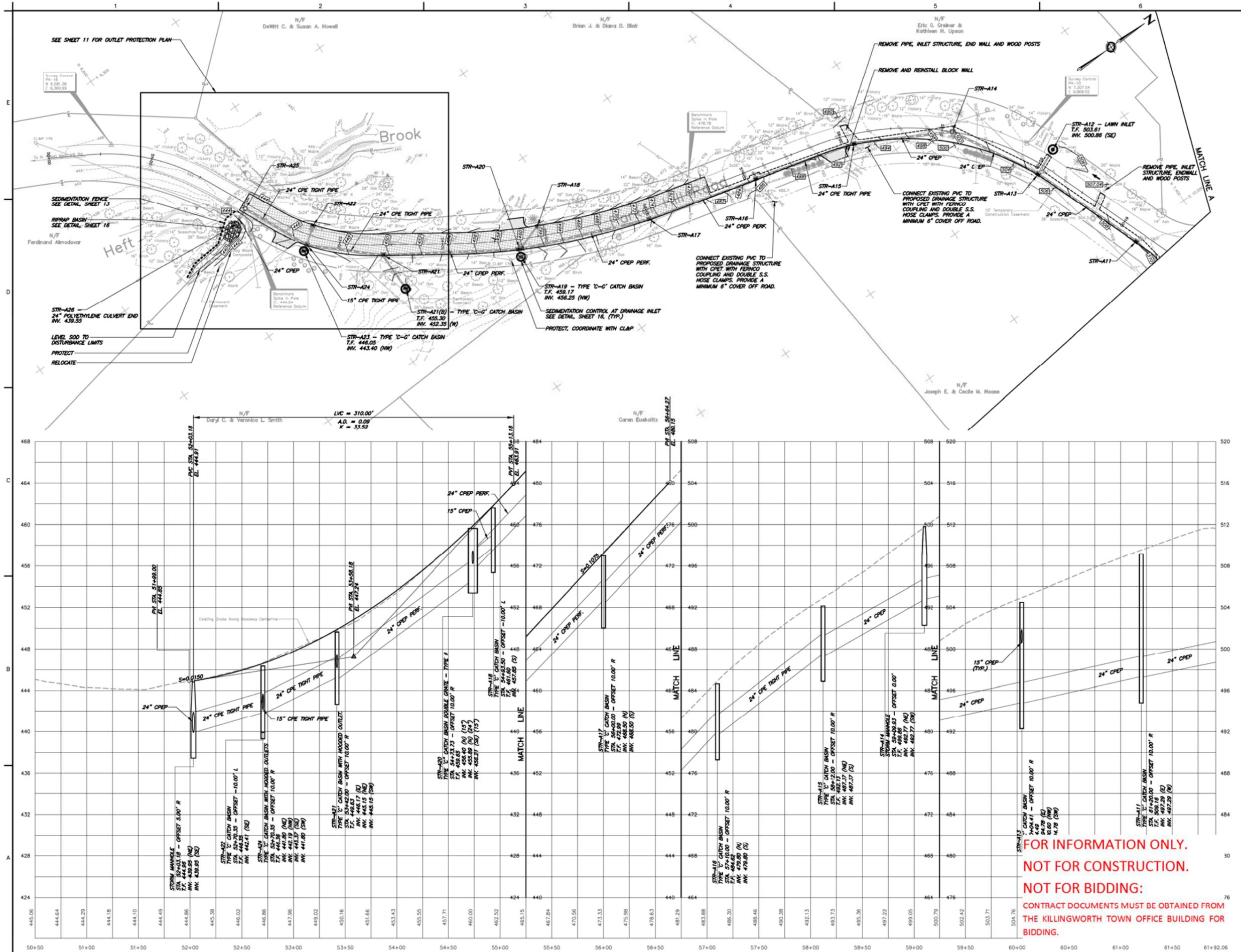
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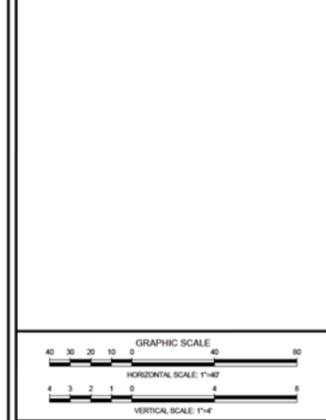
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PROJECT No.: 00550077  
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DESIGNED: ALM  
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SHEET No.:  
**5 OF 17**

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  - PROPOSED UTILITIES LOCATED OUTSIDE EDGES OF PAVEMENT AND EXISTING UTILITIES TO BE REMOVED ARE NOT SHOWN IN PROFILE VIEW.



TOWN OF  
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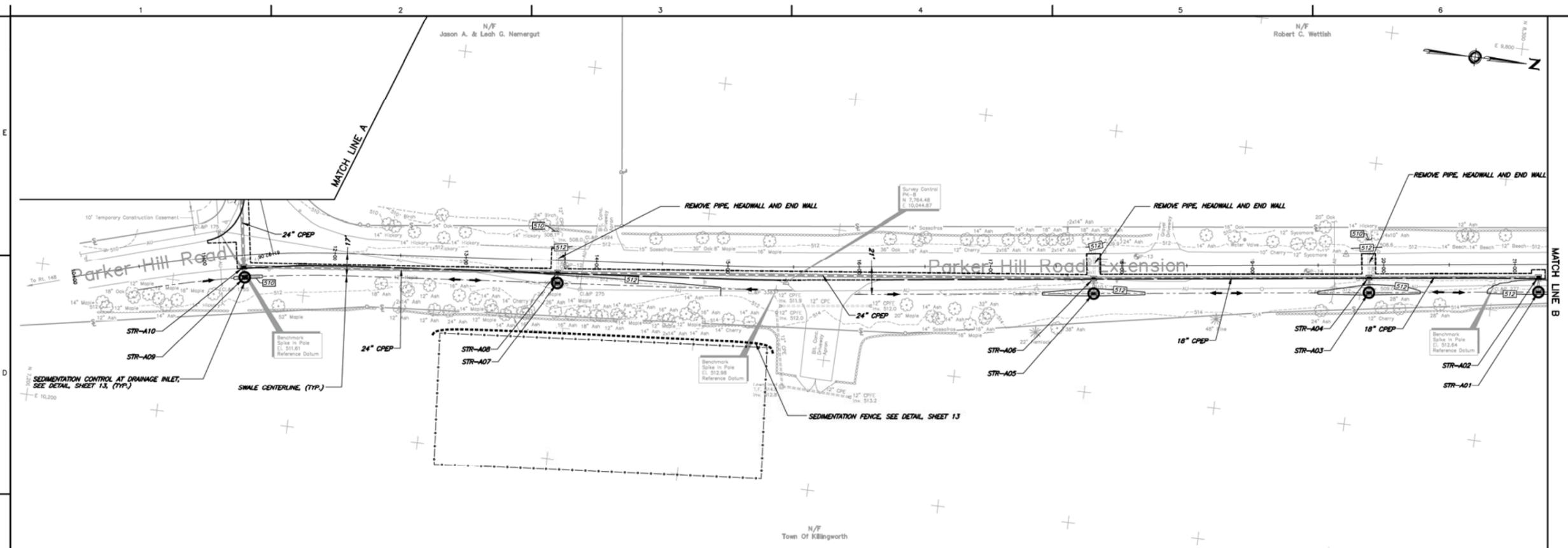
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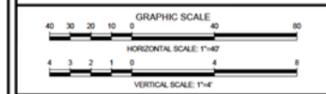
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No.	DESCRIPTION	DATE

DATE: FEBRUARY 17, 2015  
SCALE: 1"=40' H 1"=4' V  
PROJECT No.: 00550077  
CADD FILE: 00550077SP  
DESIGNED: ALM  
DRAWN: AJG  
CHECKED: JJP

SHEET No.:  
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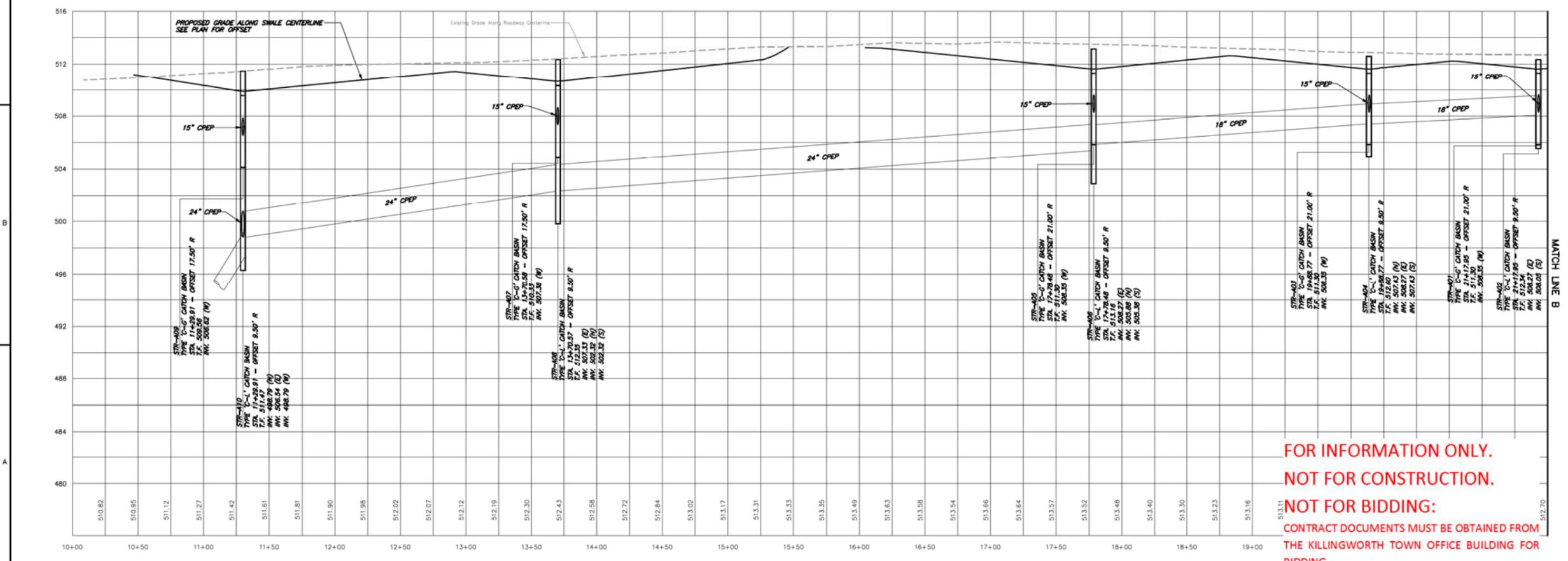
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No.	DESCRIPTION	DATE

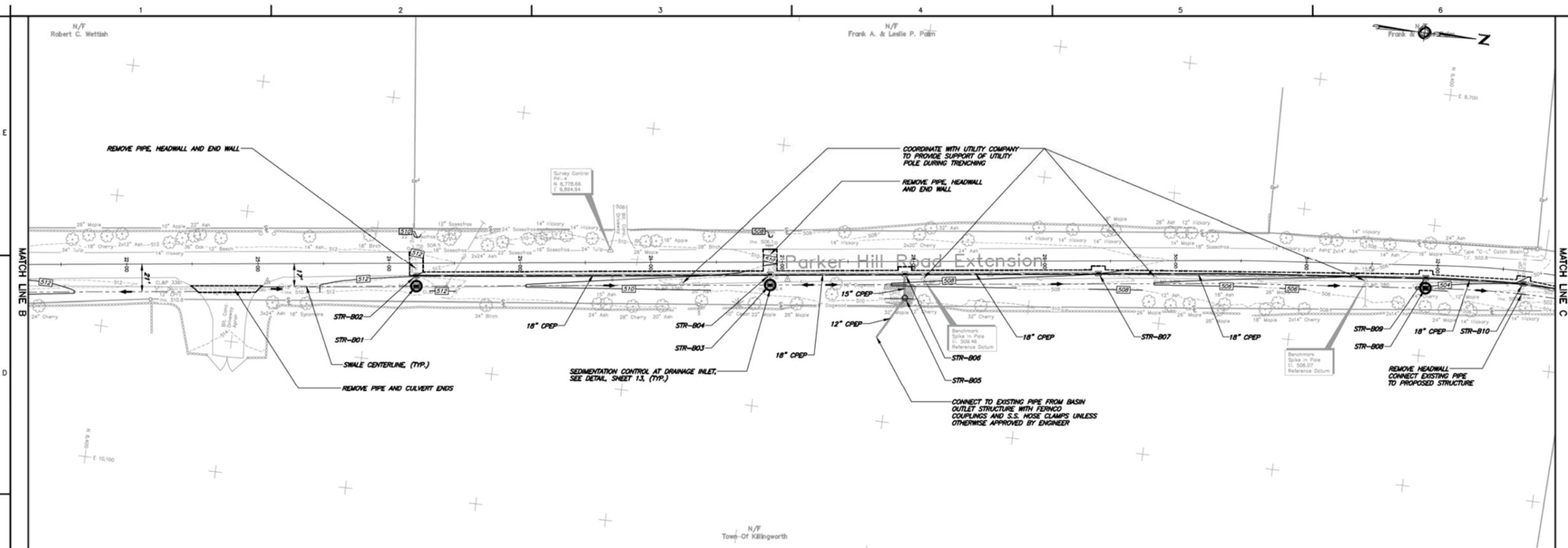
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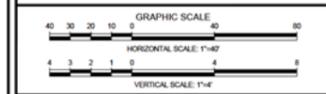
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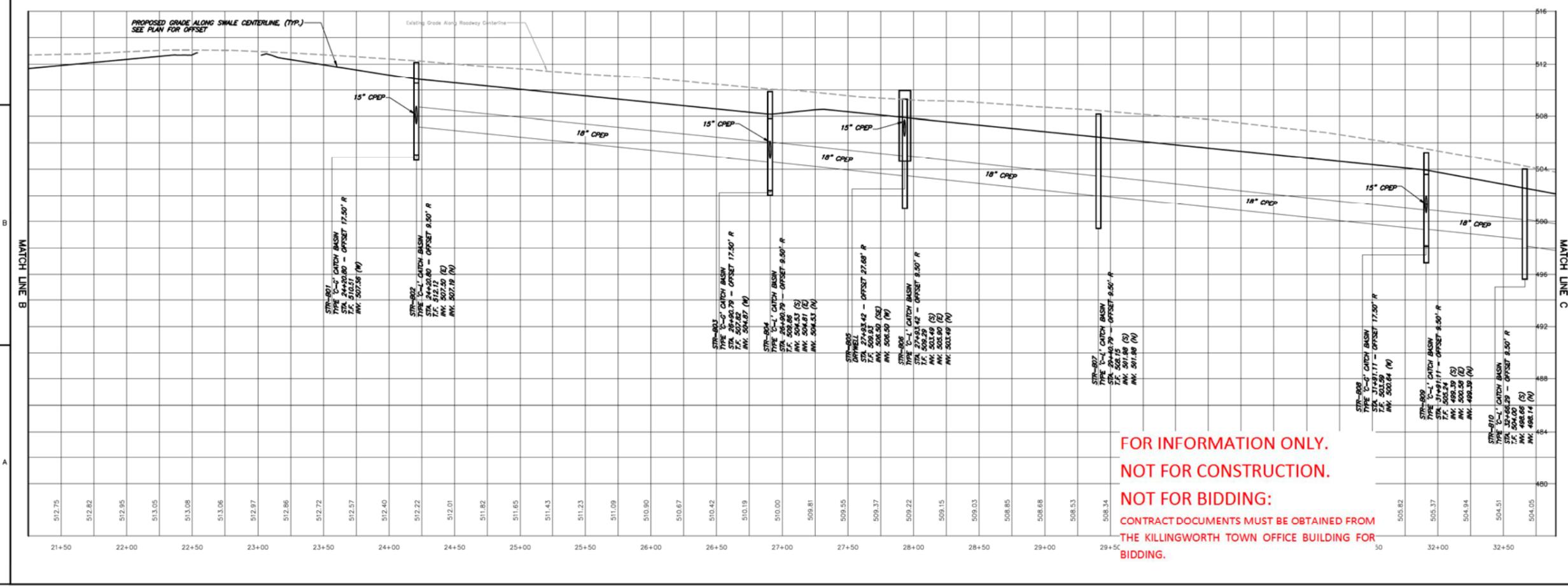
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AND EXTENSION  
DRAINAGE  
IMPROVEMENTS

PLAN AND  
PROFILE

CONTRACT DRAWINGS

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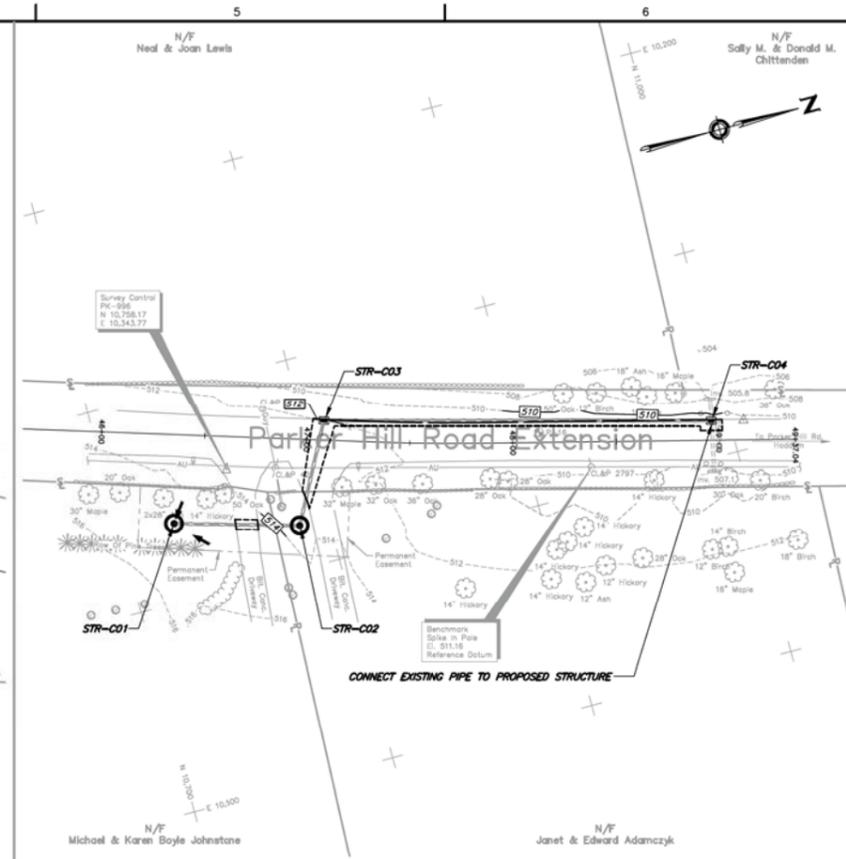
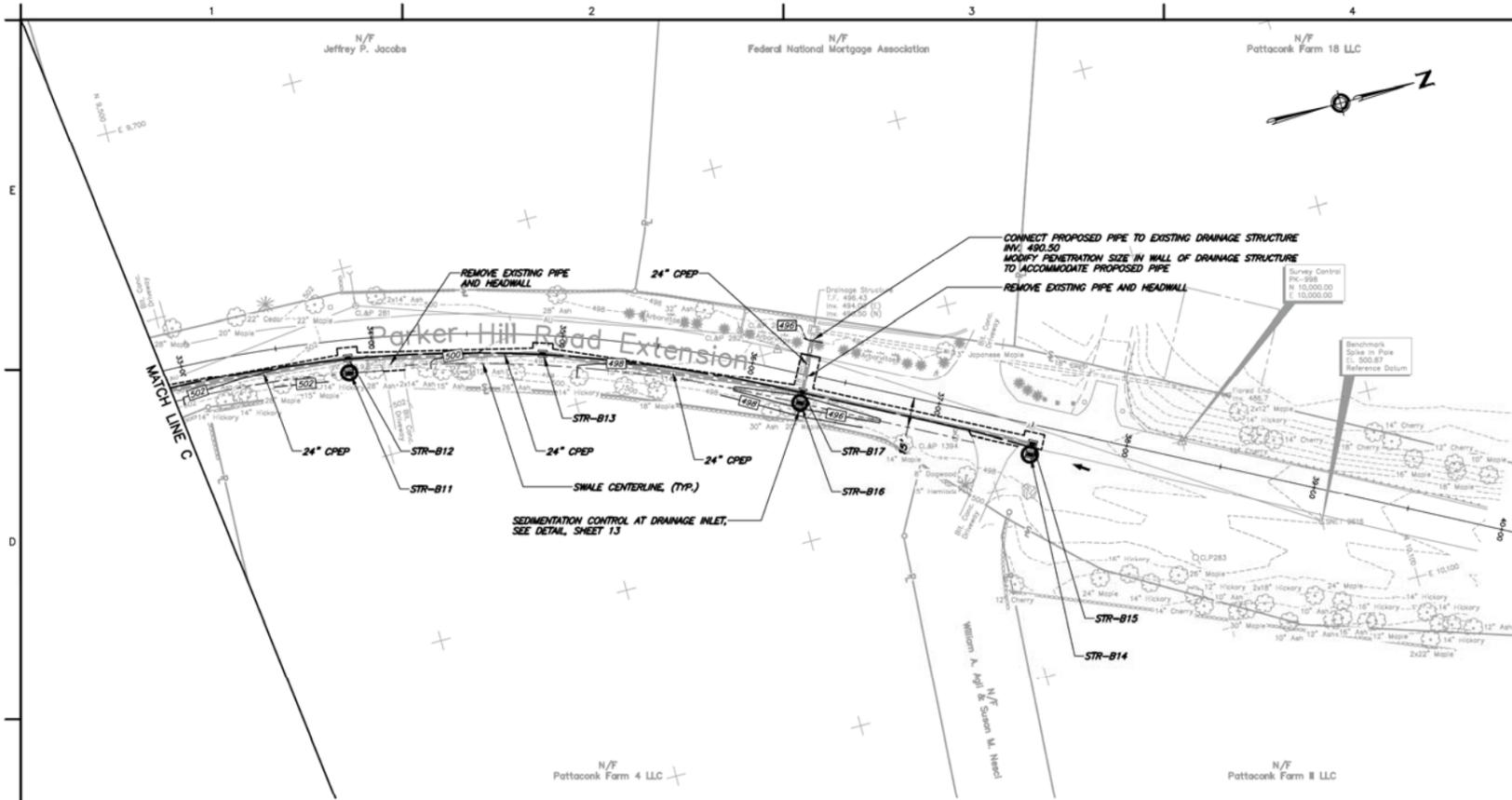
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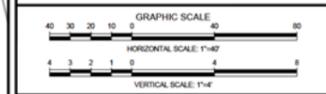
REVISIONS		
No.	DESCRIPTION	DATE

DATE: FEBRUARY 17, 2015  
SCALE: 1"=40' H 1"=4' V  
PROJECT No.: 00550077SP  
CADD FILE: 00550077SP  
DESIGNED: ALM  
DRAWN: AJG  
CHECKED: JJP

SHEET No.:  
**8 OF 17**



- NOTES:**
- In General, Lower Case Text Identifies Existing Features/Conditions.
  - In General, UPPER CASE TEXT IDENTIFIES PROPOSED FEATURES/CONDITIONS UNLESS OTHERWISE SPECIFIED.
  - FOR LOCATION OF UNDERGROUND ELECTRIC, TELEPHONE, GAS, CABLE TV AND OTHER FACILITIES OF PUBLIC UTILITY COMPANIES, INQUIRE OF "CALL BEFORE YOU DIG, INC." AT 1-800-922-4455.
  - ALL PROPOSED STORM DRAINAGE PIPES SHALL BE 15" CPEP UNLESS LABELED OTHERWISE.
  - INSTALL SEDIMENTATION CONTROL AT CATCH BASIN IN ALL PROPOSED CATCH BASINS, SEE DETAIL, SHEET 13.



TOWN OF  
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PARKER HILL ROAD  
AND EXTENSION  
DRAINAGE  
IMPROVEMENTS

PLAN AND  
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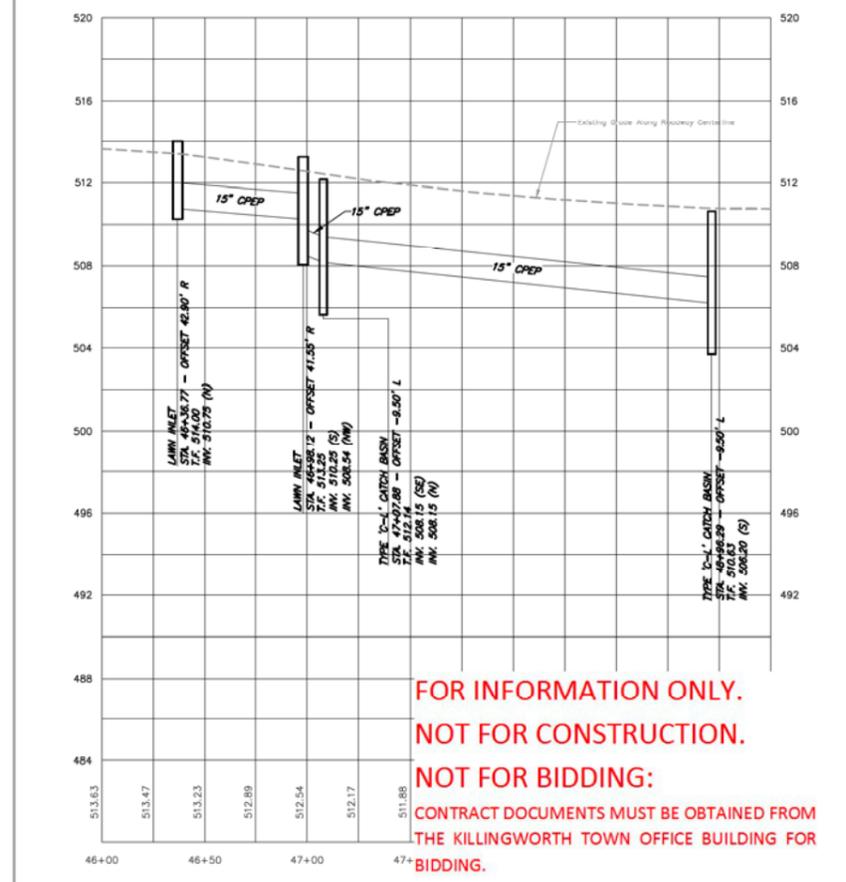
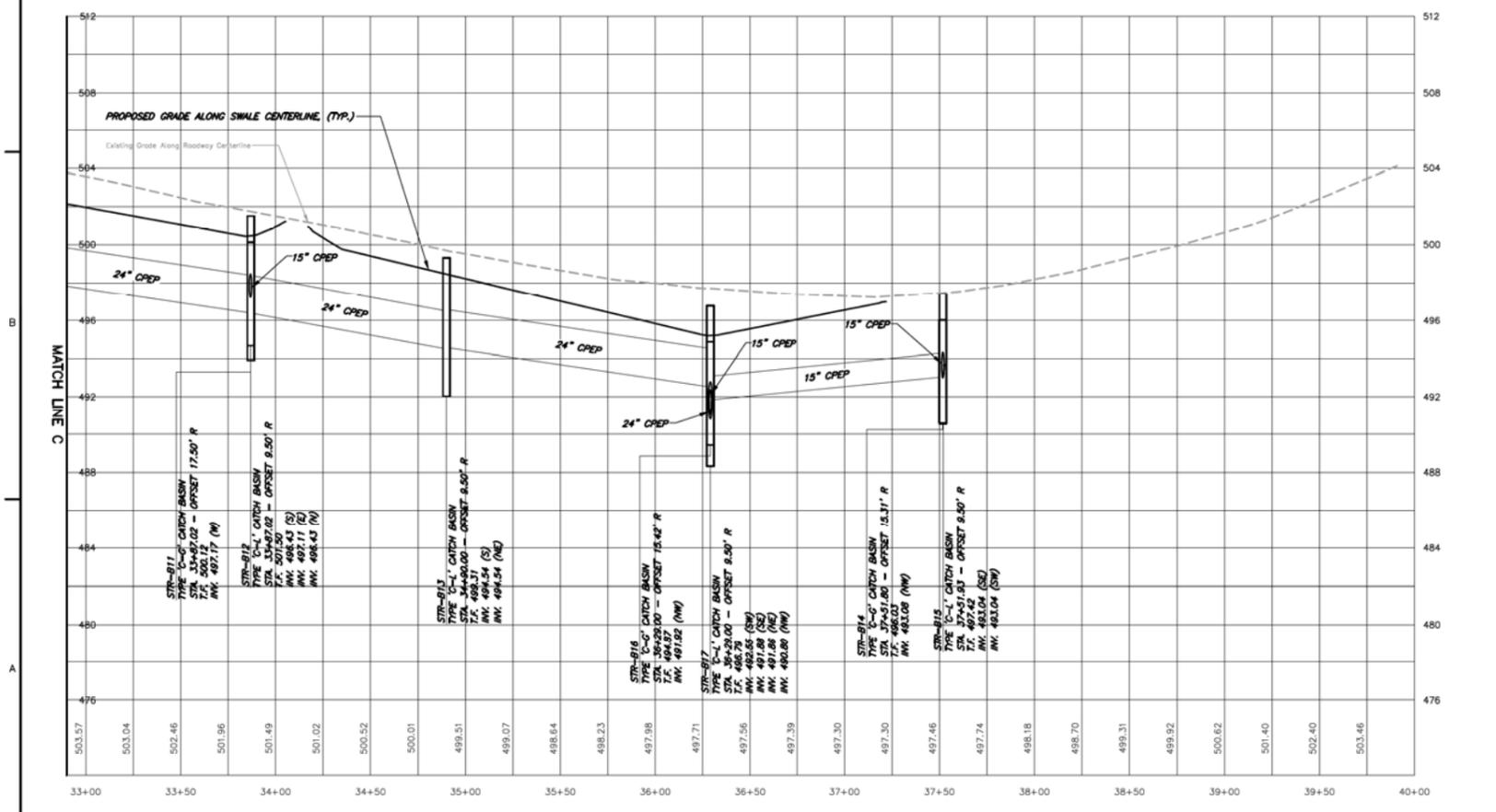
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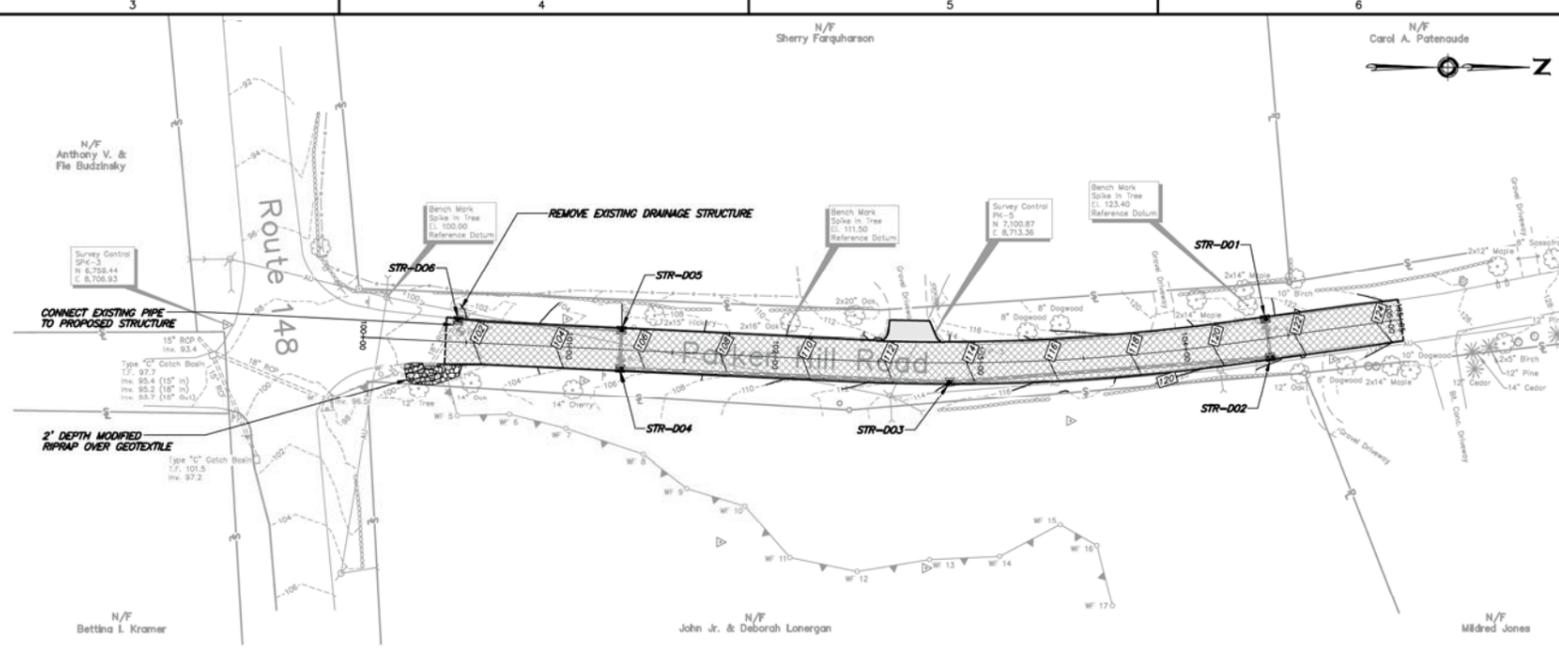
REVISIONS		
No.	DESCRIPTION	DATE

DATE: FEBRUARY 17, 2015  
SCALE: 1"=40' H 1"=4' V  
PROJECT No.: 00550077  
CADD FILE: 00550077SP  
DESIGNED: ALM  
DRAWN: AJS  
CHECKED: JJP

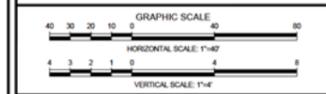
SHEET No.:  
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  - ALL PROPOSED STORM DRAINAGE PIPES SHALL BE 15" CPEP UNLESS LABELED OTHERWISE.
  - INSTALL SEDIMENTATION CONTROL AT CATCH BASIN IN ALL PROPOSED CATCH BASINS, SEE DETAIL, SHEET 13.
  - PROPOSED UTILITIES LOCATED OUTSIDE EDGES OF PAVEMENT AND EXISTING UTILITIES TO BE REMOVED ARE NOT SHOWN IN PROFILE VIEW.



TOWN OF KILLINGWORTH, CONNECTICUT  
 PARKER HILL ROAD AND EXTENSION DRAINAGE IMPROVEMENTS

PLAN AND PROFILE

CONTRACT DRAWINGS

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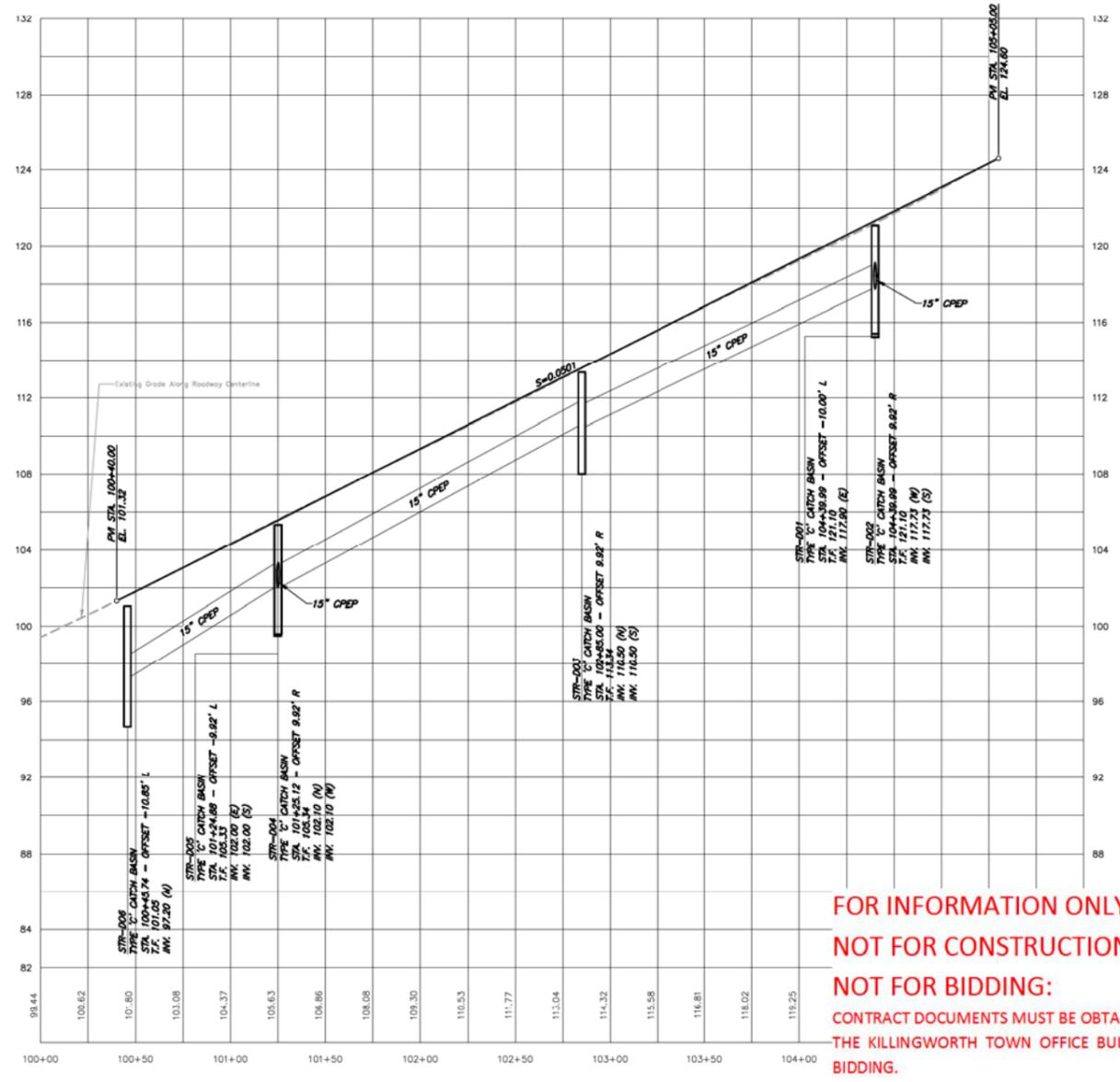
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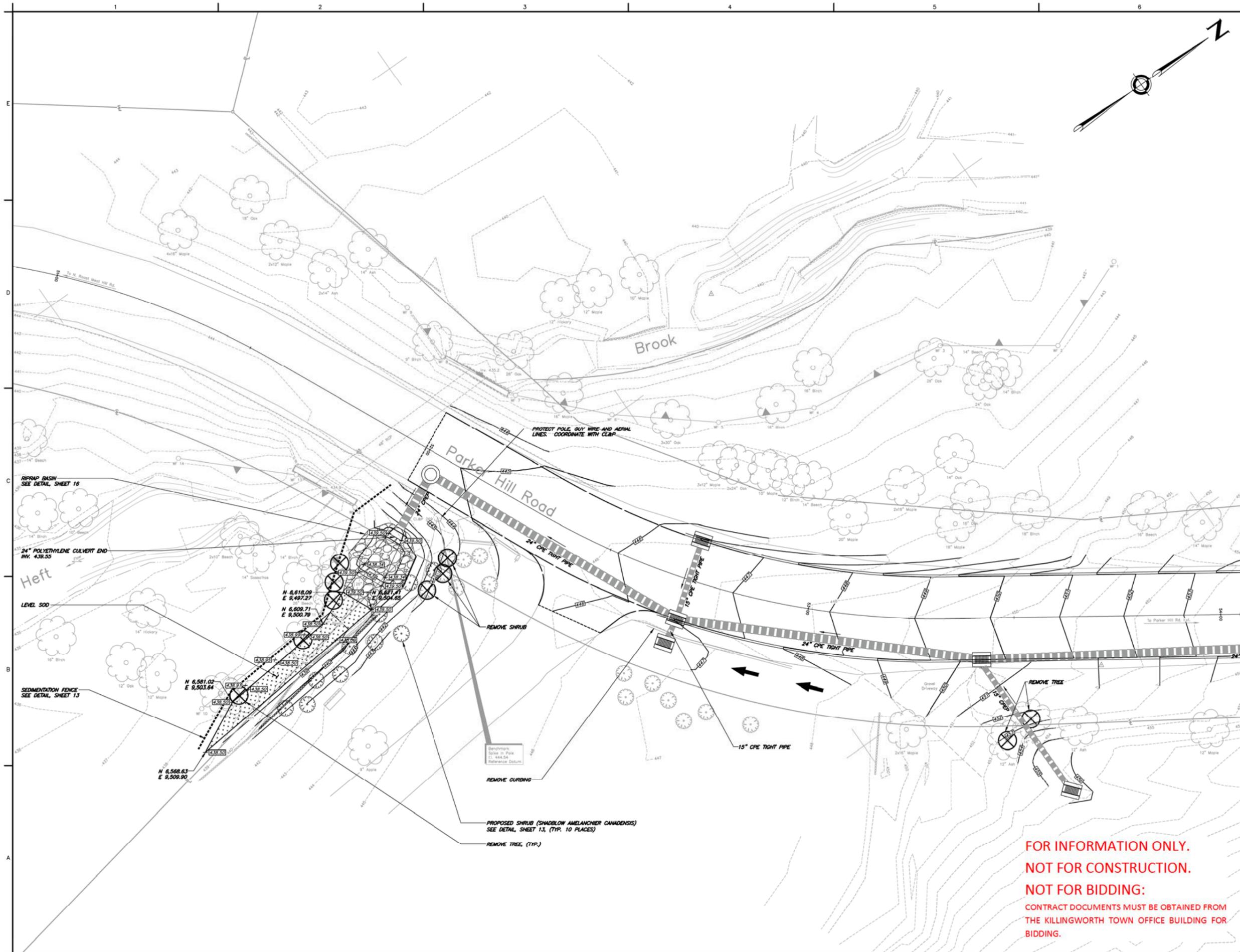
REVISIONS	
No.	DATE

DATE: FEBRUARY 17, 2015  
 SCALE: 1"=40' H 1"=4' V  
 PROJECT No.: 00550077  
 CADD FILE: 00550077SP  
 DESIGNED: ALM  
 DRAWN: AJS  
 CHECKED: JHP

SHEET No.: 10 OF 17

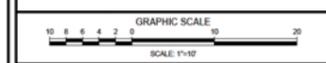


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TOWN OF  
KILLINGWORTH,  
CONNECTICUT

PARKER HILL ROAD  
DRAINAGE  
IMPROVEMENTS

OUTLET  
PROTECTION PLAN

CONTRACT DRAWINGS

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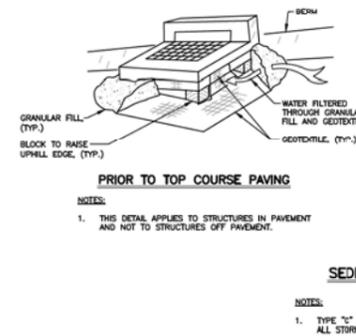
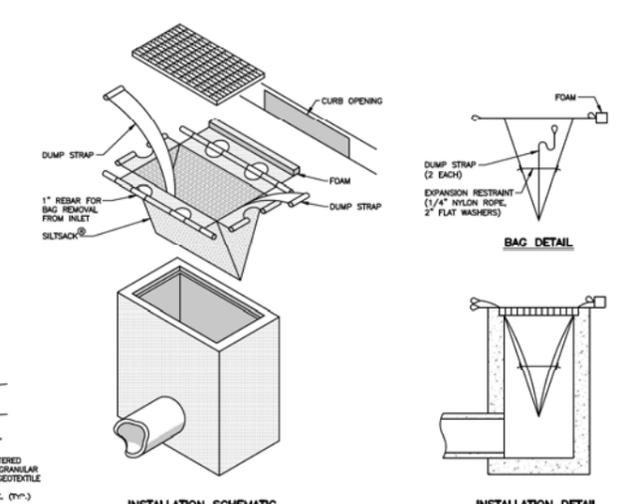
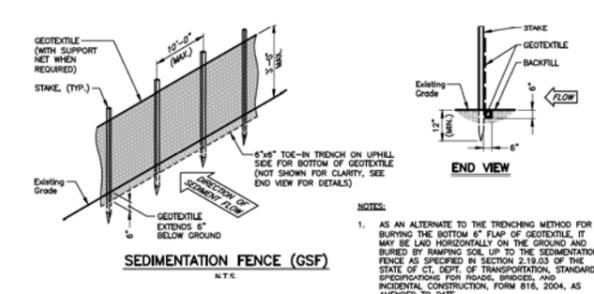
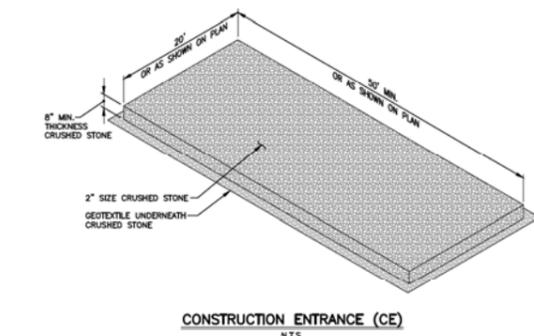
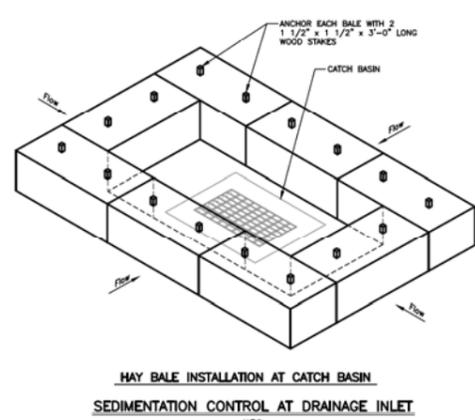
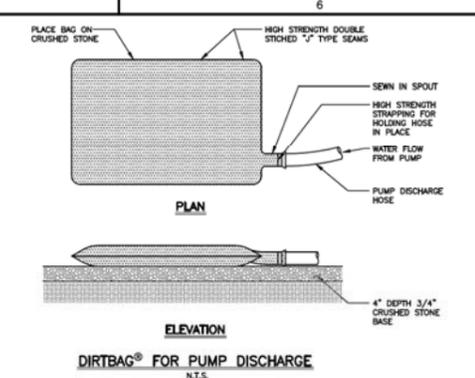
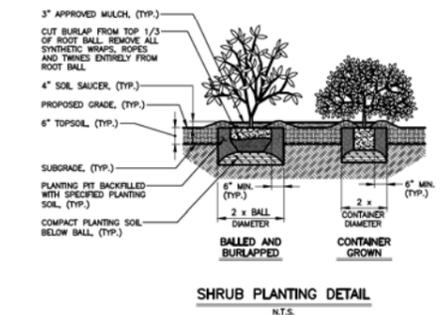
REVISIONS		
No.	DESCRIPTION	DATE

DATE: FEBRUARY 17, 2015  
SCALE: 1"=10'  
PROJECT No.: 00550077  
CADD FILE: 00550077OutletPro  
DESIGNED: ALM  
DRAWN: AJG  
CHECKED: JHP

SHEET No.:  
**11 OF 17**

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TOWN OF  
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CONNECTICUT

PARKER HILL ROAD  
AND EXTENSION  
DRAINAGE  
IMPROVEMENTS

STORMWATER  
POLLUTION CONTROL  
DETAILS

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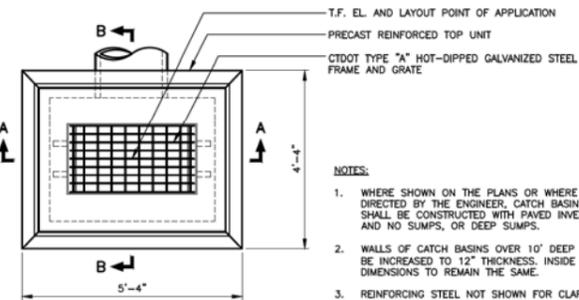
REVISIONS	
No.	DATE

DATE: FEBRUARY 17, 2015  
SCALE: AS NOTED  
PROJECT No.: 00550077  
CADD FILE: 00550077ED  
DESIGNED: ALM  
DRAWN: AJS  
CHECKED: JHP

SHEET No.:  
**13 OF 17**

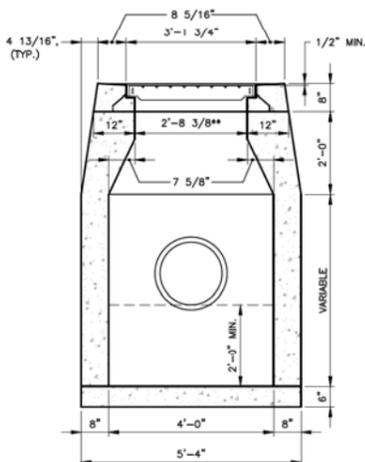
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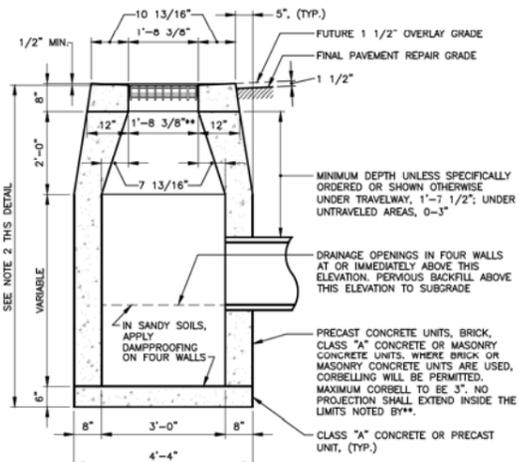
- NOTES:**
- WHERE SHOWN ON THE PLANS OR WHERE DIRECTED BY THE ENGINEER, CATCH BASINS SHALL BE CONSTRUCTED WITH PAVED INVERTS AND NO SUMPS, OR DEEP SUMPS.
  - WALLS OF CATCH BASINS OVER 10' DEEP TO BE INCREASED TO 12" THICKNESS, INSIDE DIMENSIONS TO REMAIN THE SAME.
  - REINFORCING STEEL NOT SHOWN FOR CLARITY.

**PLAN**

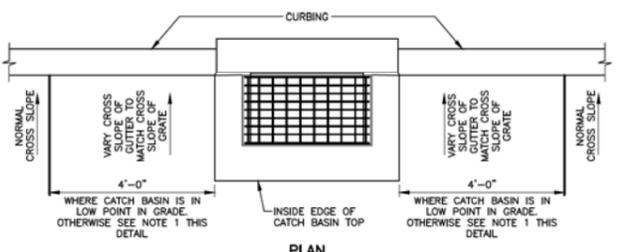


**SECTION A-A**

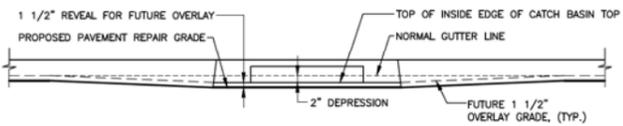
**TYPE "C-L" CATCH BASIN**  
N.T.S.



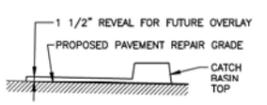
**SECTION B-B**



**PLAN**



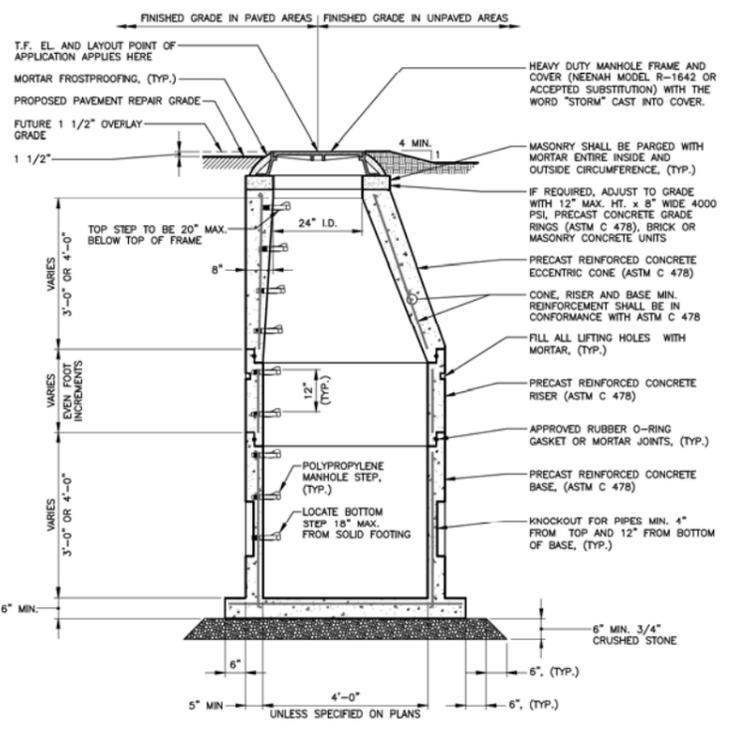
**ELEVATION**



**TYPICAL SECTION**

**BITUMINOUS CONCRETE LIP CURBING  
DEPRESSED GUTTER STRIP**  
SCALE: 1/2" = 1'-0"

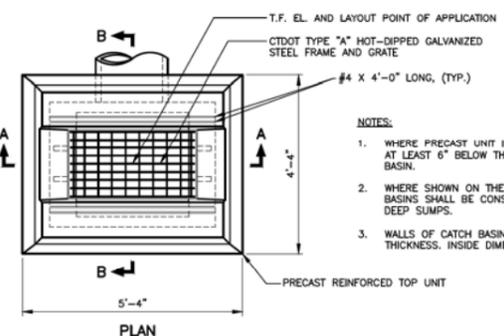
- NOTES:**
- 6'-0" ON UPGRADE SIDE OF CONTINUOUS GRADE. 1'-0" ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED.



**SECTION**

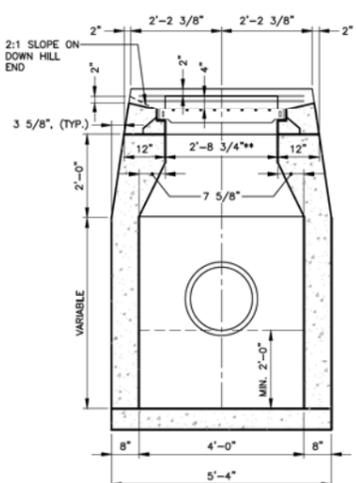
**STANDARD PRECAST STORM SEWER MANHOLE**  
N.T.S.

- NOTES:**
- WHEN 5' OR 6' DIAMETER BASES ARE SPECIFIED ON THE PLANS, PRECAST REINFORCED CONCRETE TRANSITION SECTIONS SHALL BE PLACED ABOVE THE BASES. WALL THICKNESSES SHALL INCREASE 1" FOR EACH 1' INCREASE IN INSIDE DIAMETER OF BASE.



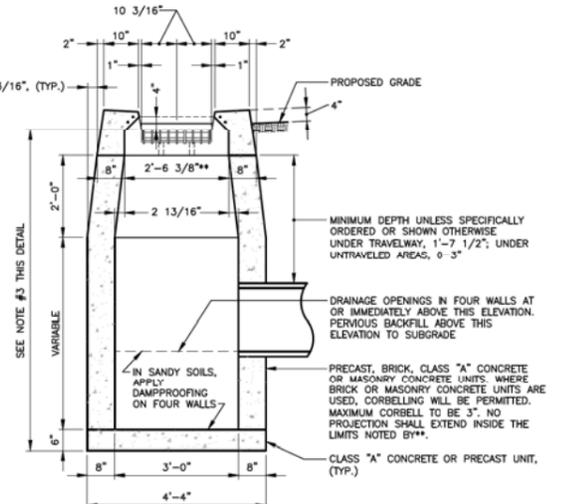
**PLAN**

- NOTES:**
- WHERE PRECAST UNIT IS USED FOR SUMP, THE TOP OF THE UNIT SHALL BE AT LEAST 6" BELOW THE BOTTOM OF THE PIPE OUTLETING FROM THE CATCH BASIN.
  - WHERE SHOWN ON THE PLANS OR WHERE DIRECTED BY THE ENGINEER, CATCH BASINS SHALL BE CONSTRUCTED WITH PAVED INVERTS AND NO SUMPS, OR DEEP SUMPS.
  - WALLS OF CATCH BASINS OVER 10' DEEP TO BE INCREASED TO 12" THICKNESS, INSIDE DIMENSIONS TO REMAIN THE SAME.

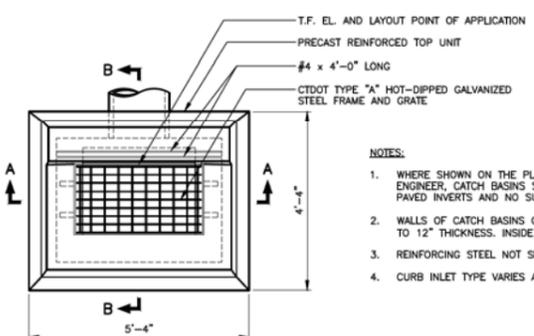


**SECTION A-A**

**TYPE "C-G" CATCH BASIN**  
N.T.S.

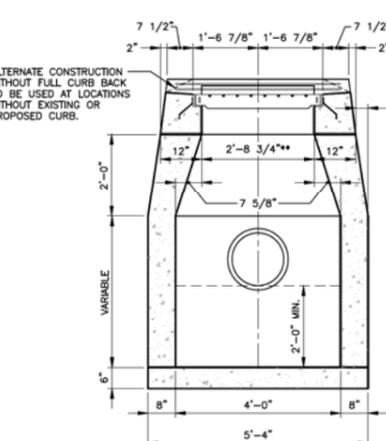


**SECTION B-B**



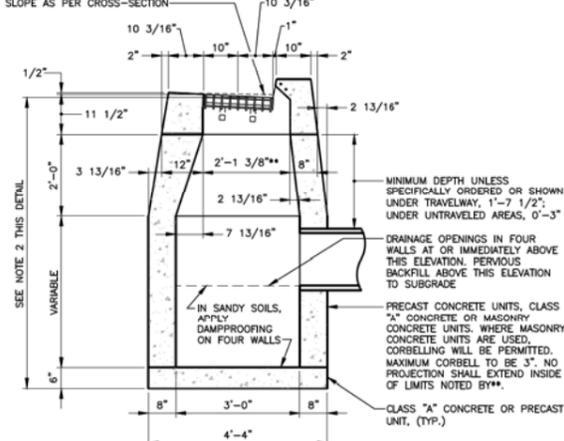
**PLAN**

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  - REINFORCING STEEL NOT SHOWN FOR CLARITY.
  - CURB INLET TYPE VARIES ACCORDING TO ADJACENT CURBING.

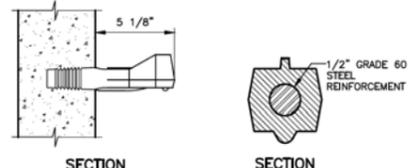


**SECTION A-A**

**TYPE "C" CATCH BASIN**  
N.T.S.

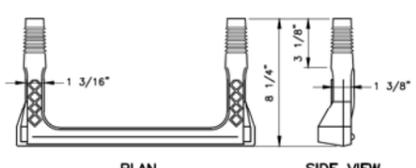


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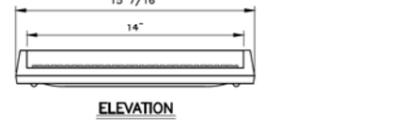
**SECTION**

**SECTION**



**PLAN**

**SIDE VIEW**



**ELEVATION**

**POLYPROPYLENE PLASTIC  
MANHOLE STEP**  
N.T.S.

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TOWN OF  
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CONNECTICUT  
  
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AND EXTENSION  
DRAINAGE  
IMPROVEMENTS

STORM DRAINAGE  
DETAILS

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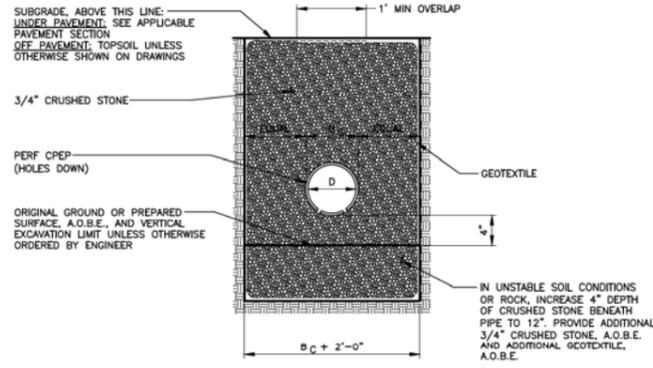
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No.	DESCRIPTION

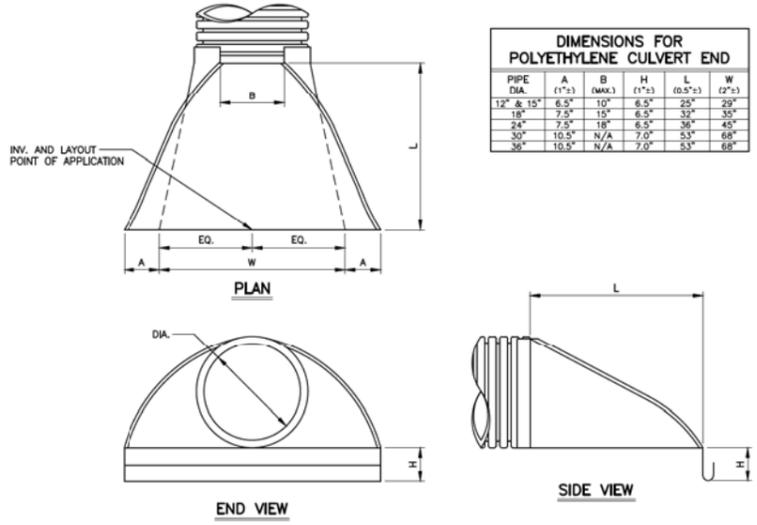
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SCALE: AS NOTED  
PROJECT No.: 00550077  
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DESIGNED: ALM  
DRAWN: AJG  
CHECKED: JJP

SHEET No.:  
**14 OF 17**

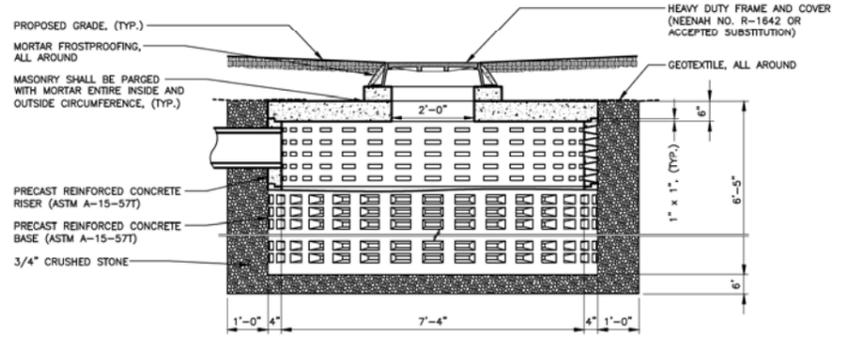


**TYPICAL TRENCH SECTION PERFORATED STORM DRAINAGE PIPE**  
N.T.S.

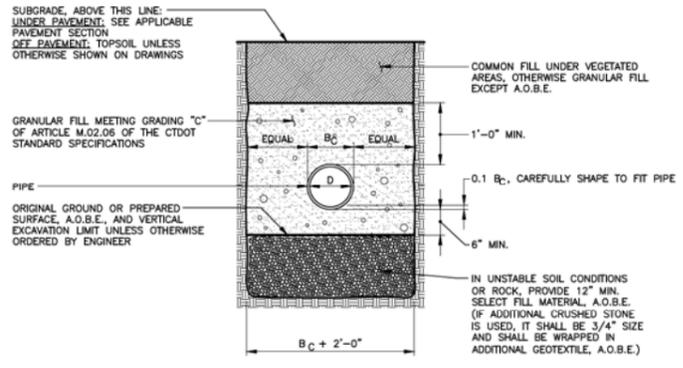
- NOTES:**
- CONTRACTOR SHALL PROTECT EXCAVATIONS BY SHORING, BRACING, SHEET PILING, UNDERPINNING OR OTHER METHODS TO PREVENT CAVE-IN OR LOOSE SOIL FROM FALLING INTO THE EXCAVATION AND DAMAGING THE WORK OR ADJACENT STRUCTURES AND UTILITIES.



**POLYETHYLENE CULVERT END**  
N.T.S.

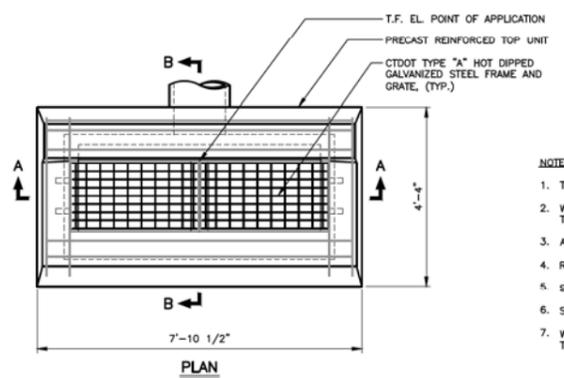


**PRECAST CONCRETE DRY WELL**  
N.T.S.

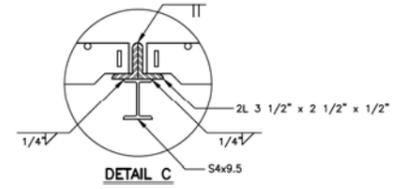


**TYPICAL TRENCH SECTION STORM DRAINAGE PIPE**  
N.T.S.

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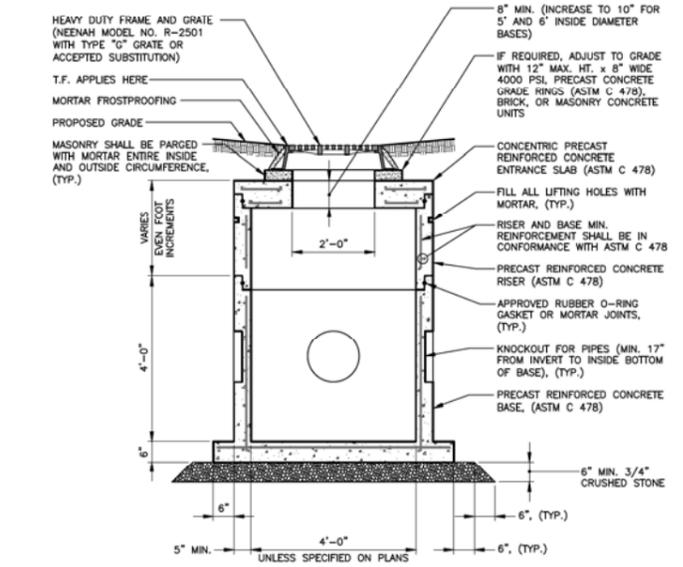


**PLAN**

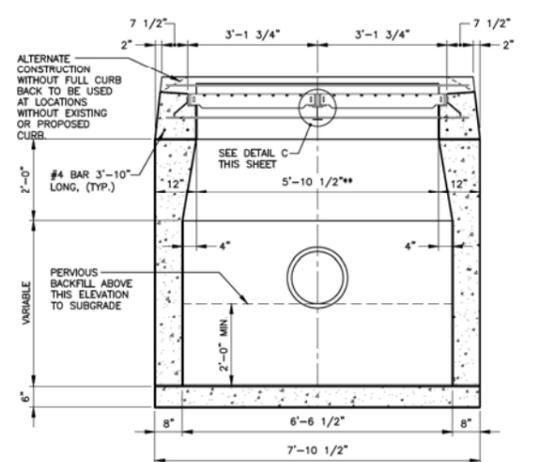


**DETAIL C**

- NOTES:**
- TWO FRAMES AND GRATES REQUIRED FOR EACH CATCH BASIN.
  - WALLS OF ALL CATCH BASINS OVER 10' DEEP TO BE INCREASED TO 12" THICKNESS. INSIDE DIMENSION TO REMAIN THE SAME.
  - ALL STEEL, EXCEPT REINFORCING BARS, SHALL BE HOT-DIPPED GALVANIZED.
  - REINFORCING BARS SHALL HAVE 2" MIN. COVER.
  - STRAIGHT REINFORCING BARS WILL BE #4 BARS.
  - STIRRUPS SHALL BE #3 BARS 9" C.C., (TYP.).
  - WHEN CATCH BASIN IS SET IN CONCRETE PAVEMENT, THE 1/2" SLOPE ON THE TOP SURFACE SHALL BE CHANGED TO MATCH ADJOINING PAVEMENT.

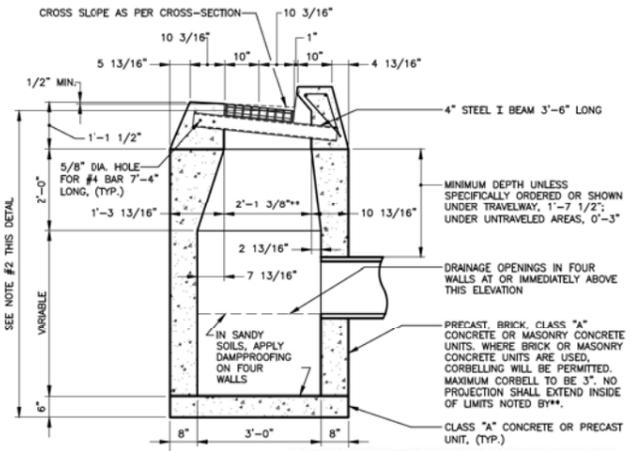


**SECTION STANDARD PRECAST CONCRETE LAWN INLET**  
N.T.S.



**SECTION A-A**

**TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE II**  
N.T.S.



**SECTION B-B**

**FOR INFORMATION ONLY.**

**NOT FOR CONSTRUCTION.**

**NOT FOR BIDDING:**

**CONTRACT DOCUMENTS MUST BE OBTAINED FROM THE KILLINGWORTH TOWN OFFICE BUILDING FOR BIDDING.**

**TOWN OF KILLINGWORTH, CONNECTICUT**  
**PARKER HILL ROAD AND EXTENSION DRAINAGE IMPROVEMENTS**

**STORM DRAINAGE DETAILS**

**CONTRACT DRAWINGS**

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Tel: (860) 526-9591 Fax: (860) 526-5416  
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Jacobson Consulting Civil and Environmental Engineers Since 1972

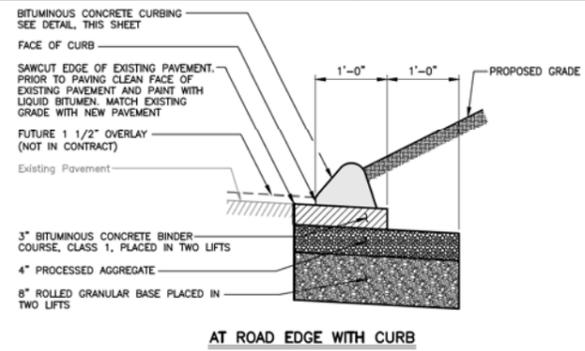
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CT REGISTRATION No. 27169

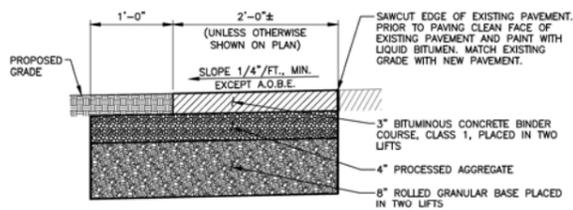
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REVISIONS		
No.	DESCRIPTION	DATE

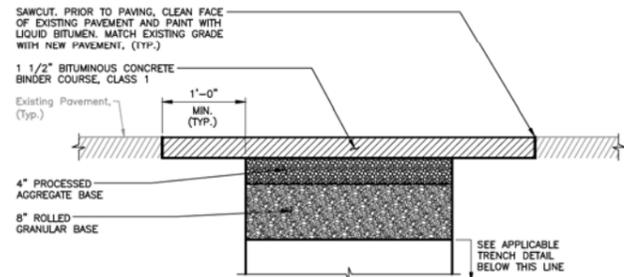
DATE: FEBRUARY 17, 2015	SHEET No.:
SCALE: AS NOTED	
PROJECT No.: 00550077	
CADD FILE: 00550077.D	
DESIGNED: ALM	15 OF 17
DRAWN: AJS	
CHECKED: JJP	



**AT ROAD EDGE WITH CURB**

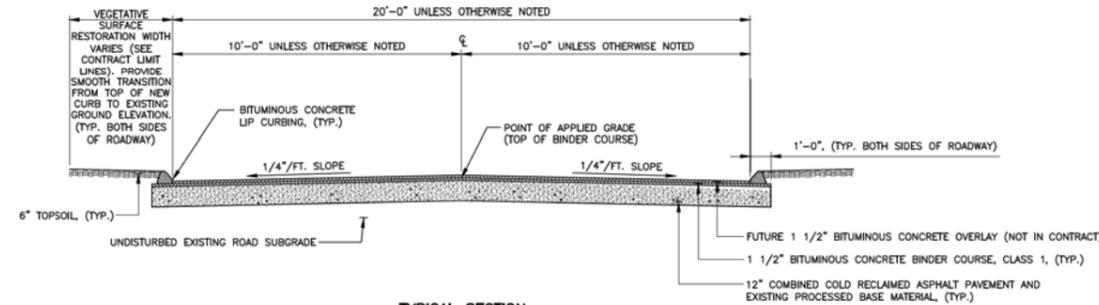


**AT ROAD EDGE WITH NO CURB**



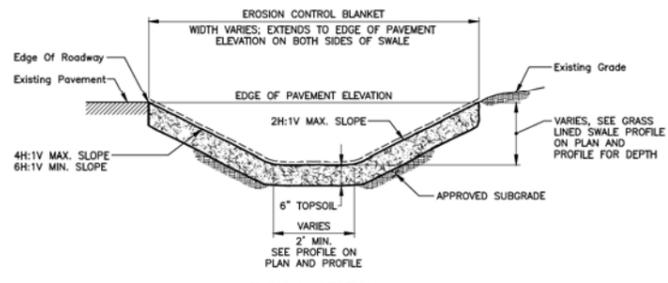
**AT UTILITY TRENCH**

**PAVEMENT REPAIR**  
N.T.S.

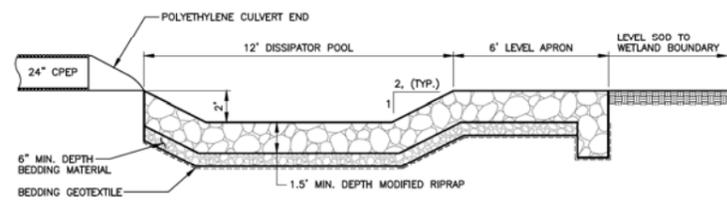


**TYPICAL SECTION**  
**PULVERIZE/RECLAIM (COLD RECLAIMED ASPHALT PAVEMENT) ROADWAY**  
N.T.S.

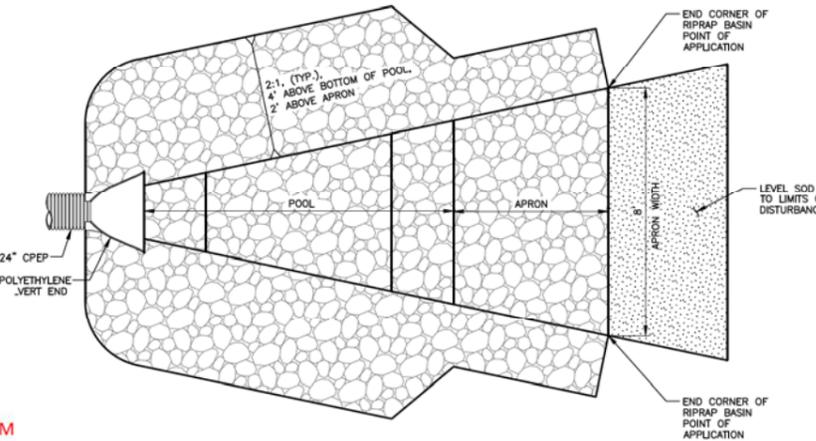
- NOTES:**
- PULVERIZE/RECLAIM THE EXISTING PAVEMENT, CURBING AND EXISTING PROCESSED BASE MATERIAL (COLD RECLAIMED ASPHALT PAVEMENT) TO A TEN (10) INCH DEPTH BELOW THE EXISTING PAVEMENT SURFACE.
  - GRADE THE EXISTING COLD RECLAIMED ASPHALT PAVEMENT TO THE PROPOSED ROADWAY BASE PROFILE WITH A TWELVE (12) INCH TOTAL COMBINED DEPTH OF COLD RECLAIMED ASPHALT PAVEMENT AND EXISTING PROCESSED BASE MATERIAL PER THIS DETAIL.
  - PROVIDE ADDITIONAL PROCESSED AGGREGATE BASE MATERIAL (A.O.B.E.) ONLY WHEN COLD RECLAIMED ASPHALT PAVEMENT AND EXISTING PROCESSED BASE MATERIAL BECOMES INSUFFICIENT IN QUALITY OR QUANTITY, AS DIRECTED BY THE ENGINEER.



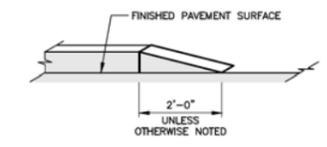
**TYPICAL SECTION**  
**GRASS LINED SWALE**  
N.T.S.



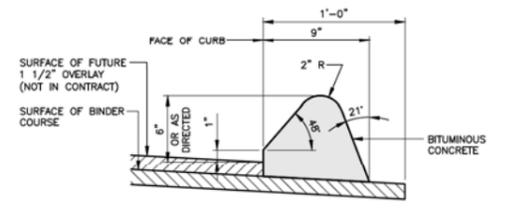
**SECTION VIEW**



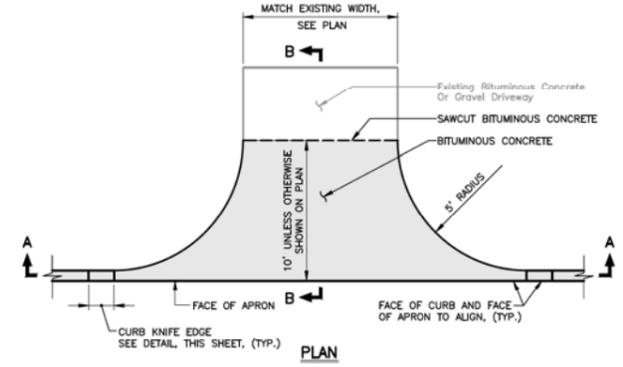
**PLAN VIEW**  
**RIPRAP BASIN DESIGN**  
N.T.S.



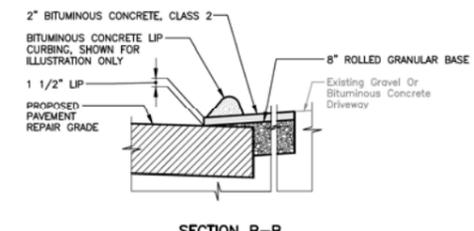
**CURB KNIFE EDGE**  
N.T.S.



**BITUMINOUS CONCRETE CURBING**  
N.T.S.



**SECTION A-A**



**SECTION B-B**  
**DRIVEWAY APRON**  
N.T.S.

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**NOT FOR BIDDING:**  
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**TOWN OF**  
**KILLINGWORTH,**  
**CONNECTICUT**

**PARKER HILL ROAD**  
**AND EXTENSION**  
**DRAINAGE**  
**IMPROVEMENTS**

**ROAD DETAILS**

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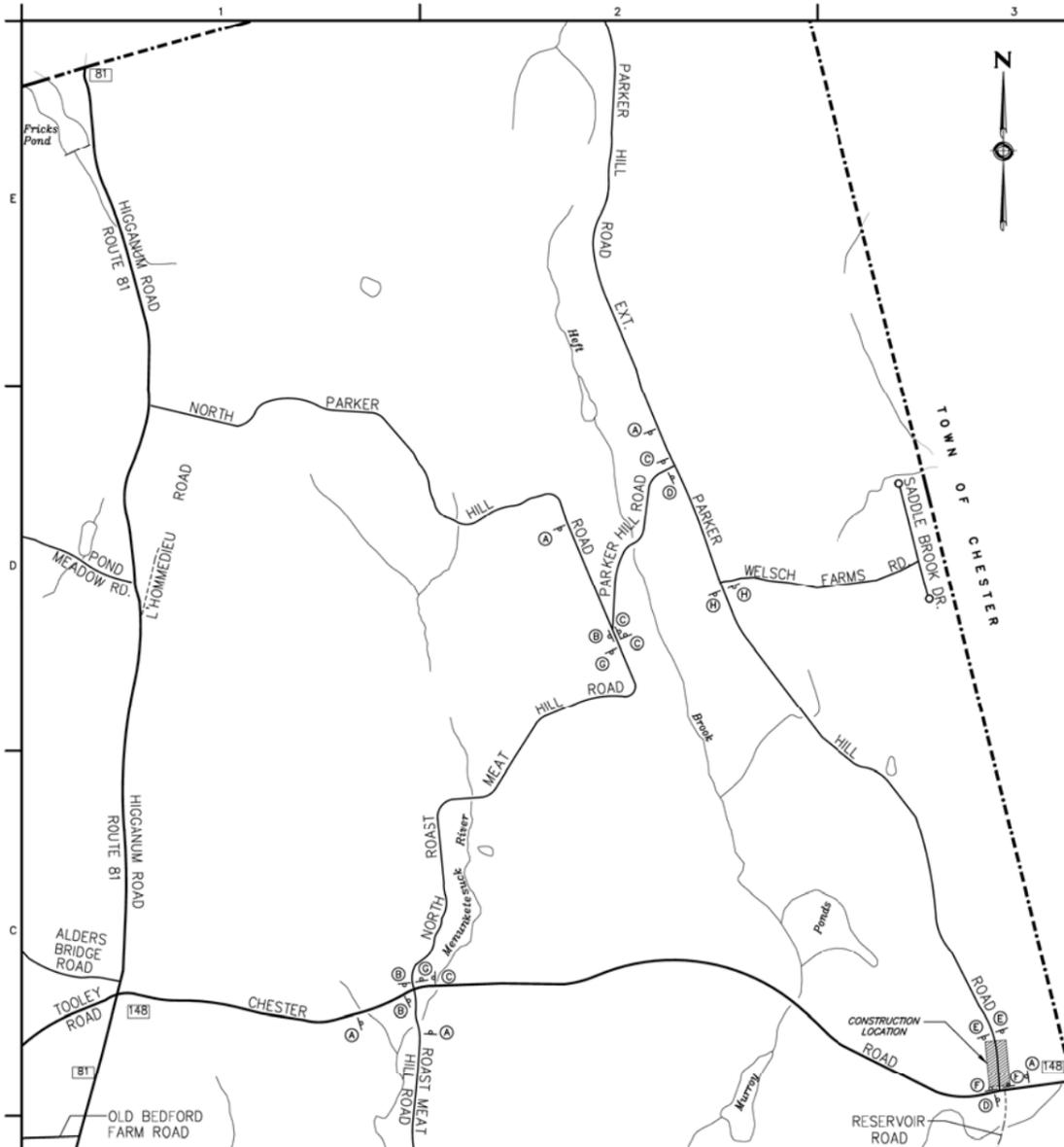
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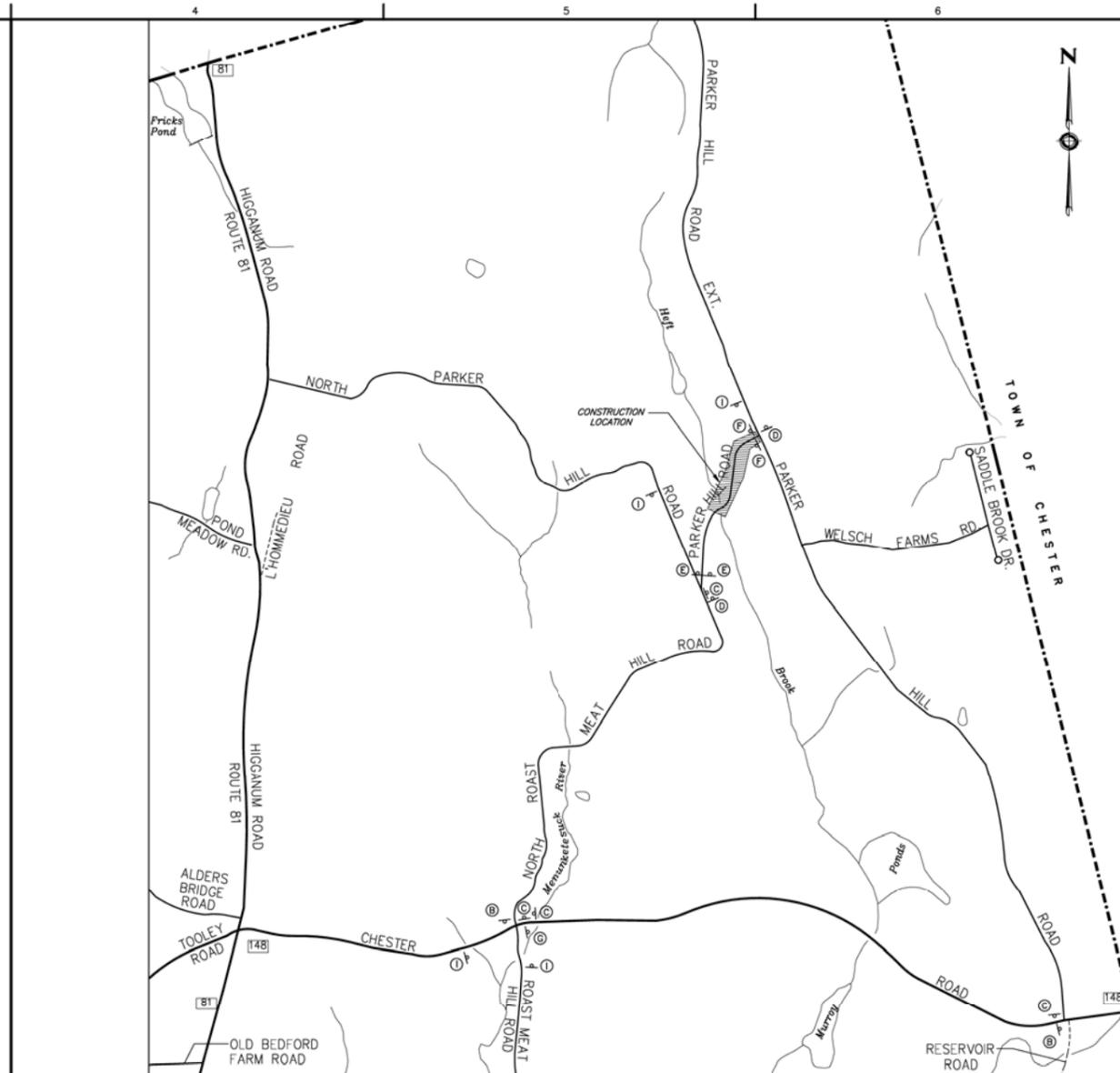
REVISIONS	
NO.	DESCRIPTION

DATE:	FEBRUARY 17, 2015	SHEET No.:	16 OF 17
SCALE:	AS NOTED		
PROJECT No.:	00550077		
CADD FILE:	00550077D1		
DESIGNED:	ALM		
DRAWN:	AJS		
CHECKED:	JMP		



**PARKER HILL ROAD NEAR ROUTE 148  
TEMPORARY CONSTRUCTION DETOUR PLAN**

SCALE: 1"=800'  
BUS TIMES THROUGH CONSTRUCTION LOCATION: 7:00 A.M. TO 8:30 A.M. AND 2:00 P.M. TO 4:00 P.M.



**PARKER HILL ROAD NEAR HEFT BROOK  
TEMPORARY CONSTRUCTION DETOUR PLAN**

SCALE: 1"=800'  
BUS TIMES THROUGH CONSTRUCTION LOCATION: 8:07 A.M. AND 3:44 P.M.

TOWN OF  
KILLINGWORTH,  
CONNECTICUT

**PARKER HILL ROAD  
AND EXTENSION  
DRAINAGE  
IMPROVEMENTS**

**TEMPORARY TRAFFIC  
CONTROL PLAN**

CONTRACT DRAWINGS

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DRAWN: AJS  
CHECKED: JHP

SHEET No.:  
**17 OF 17**

CONSTRUCTION SIGN LEGEND			
SIGN DESIGNATION	CTDOT SIGN NUMBER	DIMENSION	SIGN FACE
(A)	80-9928	60" x 30"	PARKER HILL ROAD CLOSED AT ROUTE 148 USE DETOUR
(B)	80-9913 80-9701	60" x 10" 48" x 18"	PARKER HILL ROAD ← DETOUR
(C)	80-9913 80-9702	60" x 10" 48" x 18"	PARKER HILL ROAD DETOUR →
(D)	80-9708	24" x 18"	END OF DETOUR
(E)	80-9081	60" x 30"	ROAD CLOSED TO THRU TRAFFIC

CONSTRUCTION SIGN LEGEND			
SIGN DESIGNATION	CTDOT SIGN NUMBER	DIMENSION	SIGN FACE
(F)	80-9081	60" x 30"	ROAD CLOSED TO THRU TRAFFIC
(G)	80-9701	48" x 18"	← DETOUR
(H)	80-9707	24" x 12"	DETOUR
(I)	80-9077	60" x 30"	ROAD CLOSED XX MILE AHEAD LOCAL TRAFFIC ONLY
(J)	80-9928	60" x 30"	PARKER HILL ROAD CLOSED USE DETOUR

**MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:**

- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR MAINTENANCE AND PROTECTION OF TRAFFIC, INCLUDING FURNISHING ALL NECESSARY TRAFFIC CONTROL AND SAFETY SIGNS, DEVICES, FLAGMEN, ETC.
- PORTIONS OF PARKER HILL ROAD WILL BE CLOSED DURING CONSTRUCTION OF THE PROJECT. PORTIONS OF PARKER HILL ROAD EXTENSION WILL REMAIN OPEN WITH ALTERNATING ONE-WAY TRAFFIC. THE CONTRACTOR SHALL, TO THE MAXIMUM EXTENT FEASIBLE, MINIMIZE THE PERIOD DURING WHICH PUBLIC USE OF A ROADWAY IS DISTURBED BY CONSTRUCTION ACTIVITIES. SCHEDULE ALL WORK SUCH THAT IT IS PERFORMED IN A CONTINUOUS COORDINATED SUCCESSION.
- THE "TEMPORARY CONSTRUCTION DETOUR PLAN" IS A MINIMUM GUIDELINE ONLY FOR THE MAINTENANCE AND PROTECTION OF TRAFFIC. A SPECIFIC MAINTENANCE AND PROTECTION OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION ACTIVITY.
- MAINTAIN ACCESS TO DRIVEWAYS DURING CONSTRUCTION. COORDINATE WITH PROPERTY OWNERS WITHIN CONSTRUCTION LOCATIONS.
- ONLY ONE CONSTRUCTION LOCATION SHALL BE CLOSED AT A TIME. ALL WORK REQUIRING A ROAD CLOSURE IN A CONSTRUCTION LOCATION SHALL BE COMPLETED PRIOR TO CLOSING THE OTHER CONSTRUCTION LOCATION.
- MAINTAIN ALTERNATING ONE-WAY TRAFFIC FOR SCHOOL BUSES DURING THE BUS TIMES NOTED FOR EACH PLAN UNTIL JUNE 15th AND AFTER AUGUST 26th. THERE WILL BE NO REQUIREMENT TO MAINTAIN TRAFFIC FOR SCHOOL BUSES WHEN SCHOOL IS NOT IN SESSION DUE TO, BUT NOT LIMITED TO, SCHEDULED SCHOOL HOLIDAYS OR VACATIONS.
- NOTIFY THE TOWN AT LEAST FOURTEEN (14) DAYS IN ADVANCE OF AN ANTICIPATED ROAD CLOSURE.
- THE PLACEMENT OF SIGNS SHALL NOT OBSCURE ANY PRESENT SIGNING OR SIGHTLINES FROM DRIVEWAYS OR INTERSECTING ROUTES. COORDINATE EXACT PLACEMENT OF SIGNS WITH THE ENGINEER.
- SIGNS SHALL BE PLACED ON THE SPECIFIC METAL SIGN POSTS, THEY SHALL NOT BE PLACED ON EXISTING POSTS, UTILITY POLES OR TREES.
- APPROXIMATE MILEAGE FOR SIGN (H) SHALL SHOW IN THE SIGN AND THE SIGN SHALL BE PLACED IN THE FIELD ACCORDINGLY.
- PROVIDE TYPE 'B' BARRICADE WARNING LIGHTS, HIGH INTENSITY ON SIGNS (E) AND (F).
- LOCATION AND DISTANCE BETWEEN CONSTRUCTION SIGNS MAY BE REVISED BY THE ENGINEER TO MEET FIELD CONDITIONS.
- CONSTRUCTION SIGNS THAT ARE TEMPORARILY NOT APPLICABLE SHALL BE REMOVED OR COVERED WITH AN OPAQUE COVERING.
- COMPLY WITH CTDOT STANDARD SHEETS TR-1220\_01 & TR-1220\_02 FOR SIGN FACE MATERIALS AND INSTALLATION DETAILS.

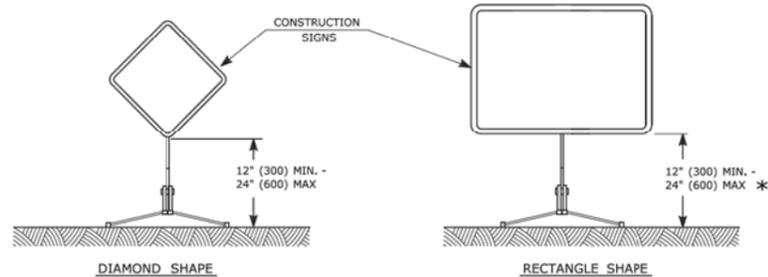
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BIDDING.

E5 - SERIES	G20 - SERIES	M4 - SERIES	R1 - SERIES	R9 & R11 - SERIES	W1 - SERIES	W3 - SERIES																																																																																												
<b>E5-1</b> <p>COPY &amp; BORDER - WHITE BACKGROUND - GREEN</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>51-6147</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	51-6147	2	<b>G20-2a</b> <p>END ROAD WORK</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>8.0</td><td>48x24</td><td>80-9512</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	8.0	48x24	80-9512	2	<b>M4-8</b> <p>DETOUR</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.0</td><td>24x12</td><td>80-9707</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24x12	80-9707	1	<b>R1-1</b> <p>STOP</p> <p>* COPY &amp; BORDER - WHITE BACKGROUND - RED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>5.19</td><td>30</td><td>31-0552</td><td>1</td></tr> <tr><td>13.30</td><td>48</td><td>31-0557</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.19	30	31-0552	1	13.30	48	31-0557	2	<b>R9-9</b> <p>SIDEWALK CLOSED</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>3.75</td><td>30x18</td><td>80-9076</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.75	30x18	80-9076	1	<b>R11-3a</b> <p>ROAD CLOSED 60 MILES AHEAD LOCAL TRAFFIC ONLY</p> <p>VARIABLE MILEAGE COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>12.5</td><td>60x30</td><td>80-9077</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60x30	80-9077	2	<b>W1-4</b> <p>BOTH LANES SHIFT LEFT</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9432L</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9432R</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9432L	1	16.0	48	80-9432R	2	<b>W3-1</b> <p>STOP AHEAD</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9808</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9808	1																			
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W4-W6 - SERIES	W8-W9 - SERIES	W13 - SERIES	W20 - SERIES	W21 - SERIES	W22 - SERIES	(VARIABLE LEGEND)	(VARIABLE LEGEND)	WHITE ORANGE																																																																																																												
<b>W4-2</b> <p>(L)</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9918L</td><td>2</td></tr> <tr><td>16.0</td><td>48</td><td>80-9917R</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9918L	2	16.0	48	80-9917R	2	<b>W8-1</b> <p>BUMP</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9901</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9902</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9901	1	16.0	48	80-9902	2	<b>W13-1</b> <p>00 M.P.H.</p> <p>SUBSTITUTE VARIABLE SPEED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.25</td><td>18</td><td>80-9568</td><td>1</td></tr> <tr><td>4.0</td><td>24</td><td>80-9569</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.25	18	80-9568	1	4.0	24	80-9569	1	<b>W20-1</b> <p>ROAD WORK AHEAD</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>6.25</td><td>30</td><td>80-9602</td><td>1</td></tr> <tr><td>9.0</td><td>36</td><td>80-9603</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9604</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	6.25	30	80-9602	1	9.0	36	80-9603	1	16.0	48	80-9604	2	<b>W20-7a</b> <p>PEDESTRIAN CROSSING</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9803</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9804</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9803	1	16.0	48	80-9804	2	<b>W21-5</b> <p>SURVEY CREW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9607</td><td>1</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9607	1	<b>W22-1</b> <p>BLASTING ZONE 1000 FT</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9620</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9625</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9620	1	16.0	48	80-9625	2	<b>(VARIABLE LEGEND)</b> <p>BLANK OR VARIABLE LEGEND</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>4.17</td><td>60x10</td><td>80-9913</td><td>2</td></tr> <tr><td>12.0</td><td>96x18</td><td>80-9914</td><td>2</td></tr> <tr><td>3.33</td><td>48x10</td><td>80-9916</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	4.17	60x10	80-9913	2	12.0	96x18	80-9914	2	3.33	48x10	80-9916	2	<b>WHITE ORANGE</b> <p>NEW SIGNAL IN OPERATION</p> <p>COPY &amp; BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>41-0620</td><td>2</td></tr> </table>	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	41-0620	2
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<b>METRIC CONVERSION CHART</b> (1" = 25mm) <table border="1"> <tr><th>ENGLISH</th><th>METRIC</th><th>ENGLISH</th><th>METRIC</th></tr> <tr><td>12"</td><td>300</td><td>60"</td><td>1500</td></tr> <tr><td>18"</td><td>450</td><td>66"</td><td>1650</td></tr> <tr><td>24"</td><td>600</td><td>72"</td><td>1800</td></tr> <tr><td>30"</td><td>750</td><td>78"</td><td>1950</td></tr> <tr><td>36"</td><td>900</td><td>84"</td><td>2100</td></tr> <tr><td>42"</td><td>1050</td><td>90"</td><td>2250</td></tr> <tr><td>48"</td><td>1200</td><td>96"</td><td>2400</td></tr> <tr><td>54"</td><td>1350</td><td></td><td></td></tr> </table>	ENGLISH	METRIC	ENGLISH	METRIC	12"	300	60"	1500	18"	450	66"	1650	24"	600	72"	1800	30"	750	78"	1950	36"	900	84"	2100	42"	1050	90"	2250	48"	1200	96"	2400	54"	1350			<b>NOT TO SCALE</b>	<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION File Name: CTDOT-TRAFFIC-STD.dgn Model: TR-1220_01	SUBMITTED BY: Charles S. Harlow NAME/DATE/TIME: 2009.09.11 14:58:43 -04'00' APPROVED BY: John F. Carey NAME/DATE/TIME: 2009.09.16 08:33:54 -04'00'	<b>CTDOT</b> <b>STANDARD SHEET</b> <b>OFFICE OF ENGINEERING</b>	<b>STANDARDS SHEET TITLE:</b> <b>SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS</b>	<b>STANDARD SHEET NO.:</b> <b>TR-1220_01</b>																																																																										
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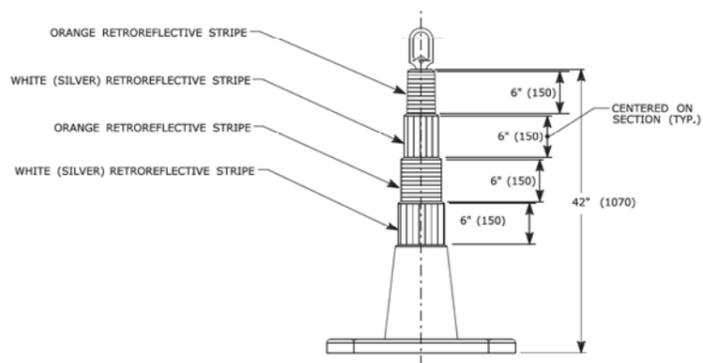
REV.	DATE	REVISION DESCRIPTION



**CONSTRUCTION SIGNS**

NOTES FOR PORTABLE SIGN SUPPORTS:

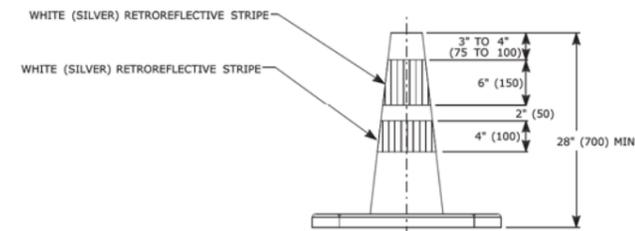
1. SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
  2. MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" (300) AND A MAXIMUM OF 24" (600). SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
  3. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
  4. PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3).
- \* FOR EXIT SIGNS, USE MIN. 72" (1800).



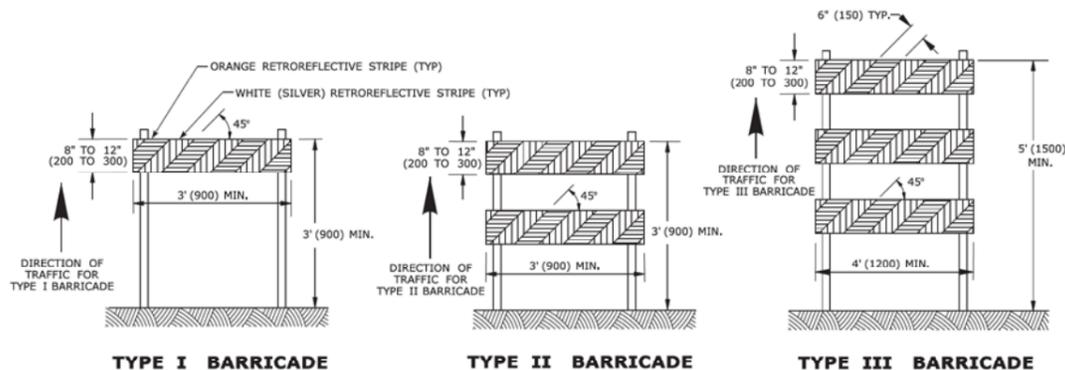
**42" (1m) TRAFFIC CONE**

NOTES:

1. TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.



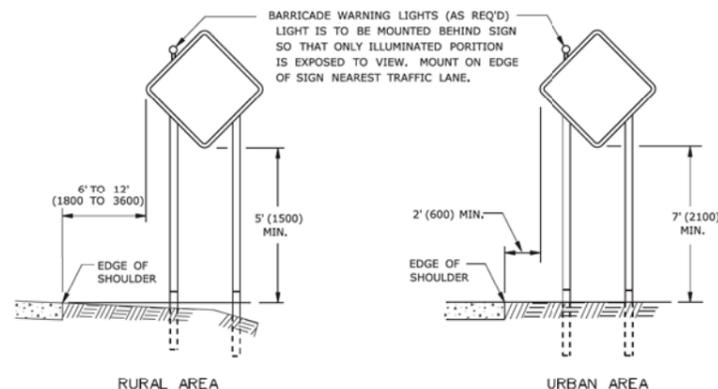
**TRAFFIC CONE**



**CONSTRUCTION BARRICADES**

NOTES:

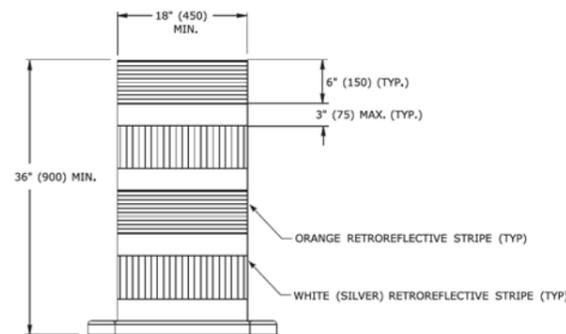
1. CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" (150) WIDE STRIPES SHALL BE USED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. RAILS FOR TYPE I AND TYPE II BARRICADES SHALL BE RETROREFLECTIVE ON BOTH SIDES. WHERE TRAFFIC PASSES ONLY IN ONE DIRECTION OF TRAVEL, ONLY THE SIDE FACING TRAFFIC SHALL BE RETROREFLECTIVE.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
5. CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
6. SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



**PLACEMENT OF CONSTRUCTION SIGNS  
TYPICAL LONG TERM INSTALLATION**

NOTES:

- SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.  
SEE TYPICAL SHEETS:  
"TYPICAL SIGN SUPPORT AND SIGN PLACEMENT DETAILS-GORE EXIT SIGN"  
"TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS"



**TRAFFIC DRUM  
FRONT VIEW**

NOTES:

1. TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
4. THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. Plotted Date: 9/11/2009		NOT TO SCALE	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	SUBMITTED BY: Charles S. Harlow NAME/DATE/TIME: 2009.09.11 14:59:23 -04'00' APPROVED BY: John F. Carey NAME/DATE/TIME: 2009.09.16 08:34:26 -04'00'	CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: <b>CONSTRUCTION SIGN SUPPORTS &amp; CHANNELIZING DEVICES</b>	STANDARD SHEET NO.: TR-1220_02
REV.	DATE	REVISION DESCRIPTION					